

BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION

CITY OF PEWAUKEE WAUKESHA COUNTY, WISCONSIN

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SHEET NO.	LOCATION	DESCRIPTION
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PROPOSED WATER MAIN

SW-01	IN: BLUEMOUND ROAD/CTH "JJ" PROPOSED WATER MAIN PLAN	FROM: 300' WEST OF EASTMOUND DRIVE TO: 200' EAST OF EASTMOUND DRIVE
SW-02R-1	IN: BLUEMOUND ROAD/CTH "JJ" PROPOSED WATER MAIN PLAN	FROM: 200' EAST OF EASTMOUND DRIVE TO: TAKOMA DRIVE
SW-03R-1	IN: BLUEMOUND ROAD/CTH "JJ" PROPOSED WATER MAIN PLAN	FROM: TAKOMA DRIVE TO: 500' EAST OF TAKOMA DRIVE
SW-04	IN: BLUEMOUND ROAD/CTH "JJ" PROPOSED WATER MAIN PLAN	FROM: 500' EAST OF TAKOMA DRIVE TO: 50' WEST OF SARATOGA DRIVE
SW-05	IN: BLUEMOUND ROAD/CTH "JJ" PROPOSED WATER MAIN PLAN	FROM: 50' WEST OF SARATOGA DRIVE TO: 450' EAST OF SARATOGA DRIVE
SW-06R-1	IN: TAKOMA DRIVE PROPOSED WATER MAIN PLAN	FROM: 200' SOUTH OF BLUEMOUND ROAD/CTH "JJ" TO: BLUEMOUND ROAD/CTH "JJ"
SW-07		WATER MAIN CONNECTIONS - EASTMOUND DRIVE & SARATOGA DRIVE

CONSTRUCTION DETAILS

DT-01 TO DT-03	CONSTRUCTION DETAILS
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GENERAL NOTES

1. FOLLOW REQUIREMENTS OF WDNR AND CITY OF PEWAUKEE TECHNICAL STANDARDS.
2. INSPECTIONS OF ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE DONE ONCE PER WEEK PRIOR TO ANY FORECAST PRECIPITATION EVENTS AND AFTER EVERY PRECIPITATION EVENT OF 1/2" OR GREATER.
3. CONTRACTOR SHALL REPAIR DEFICIENT EROSION AND SEDIMENT CONTROL MEASURES WITHIN 24-HOURS AFTER INSPECTION. ADDITIONAL EROSION AND SEDIMENT CONTROL DEVICES NOT SHOWN ON THIS PLAN MAY BE NECESSARY AS DIRECTED BY OWNER AND/OR ENGINEER.
4. ADDITIONAL EROSION AND/OR SEDIMENT CONTROL MEASURES MAY BE NECESSARY AS A RESULT OF THE CONTRACTORS METHODS.
5. CONTRACTOR SHALL NOTIFY AND OBTAIN WRITTEN ACCEPTANCE FROM ENGINEER OF PROPOSED CHANGES TO THE EROSION PLAN AND/OR SEQUENCE PRIOR TO IMPLEMENTING THE CHANGE. ANY CHANGES WILL REQUIRE DNR APPROVAL.
6. ENGINEER IS UNDER NO OBLIGATION TO ALTER EROSION PLAN AND/OR SEQUENCE.
7. CONTRACTOR SHALL SWEEP ROAD DAILY IF NECESSARY TO REMOVE SEDIMENT TRACKED ONTO THE ROADS.
8. CONSTRUCT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROLS IN ACCORDANCE WITH THE WISCONSIN DNR TECHNICAL STANDARDS AND EROSION CONTROL SPECIFICATIONS SECTION 01 57 13 OF THE PROJECT MANUAL.
9. CONTRACTOR IS RESPONSIBLE FOR FOLLOWING THE CONSTRUCTION SEQUENCE AND FOR MAINTAINING AND REPAIRING EROSION AND SEDIMENT CONTROL DEVICES.
10. CONTRACTOR TO NOTIFY OWNER OF ALL FILL AND BORROW SITES. CONTRACTOR IS SOLELY RESPONSIBLE FOR OBTAINING AND COMPLYING WITH NECESSARY EROSION CONTROL PERMITS AND FOR MAINTAINING PROPER EROSION CONTROL MEASURES ON THOSE SITES. DISPOSAL LOCATION(S) MAY NEED DNR APPROVAL PRIOR TO COMMENCING WORK.
11. EXCESS EXCAVATED MATERIAL SHALL BE REMOVED FROM SITE AND LEGALLY DISPOSED OF AT LOCATION SECURED BY CONTRACTOR.
12. EXCESS FILL/BORROW MATERIAL OR SPOILS KEPT ON SITE SHOULD BE STOCKPILED ON UPLAND AREAS AN ADEQUATE DISTANCE AWAY FROM WETLANDS AND THE WATERWAYS. PILES OF STOCKPILED SOIL SHALL BE PROTECTED AGAINST EROSION AND SHALL NOT CREATE NUISANCE DUST EMISSIONS.
13. THE LOCATION OF EXISTING UTILITIES AND OBJECTS ARE SHOWN FROM AVAILABLE INFORMATION AND THE LOCATION AS SHOWN SHALL NOT BE CONSIDERED AS ACCURATE. THE CONTRACTOR SHALL ASSUME SOLE AND FULL RESPONSIBILITY FOR ALL UTILITIES OR OBJECTS WHETHER OR NOT SHOWN ON THE PLANS AND REALIZE THAT THEIR ACTUAL LOCATION MAY BE DIFFERENT FROM THAT SHOWN. CONTRACTOR SHALL CALL DIGGER'S HOTLINE TO REQUEST UTILITY LOCATION PRIOR TO CONSTRUCTION.

CONSTRUCTION SEQUENCE

1. INSTALL WORK ZONE TRAFFIC CONTROL MEASURES AS APPROVED BY CITY OF PEWAUKEE AND WAUKESHA COUNTY.
2. CONSTRUCT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL PRACTICES IN ACCORDANCE WITH THE "WISCONSIN DNR TECHNICAL STANDARDS."
3. BEGIN CONSTRUCTION ACTIVITIES AS SHOWN ON DRAWINGS AND SPECIFICATIONS.
4. COMPLETE WATER MAIN AND SANITARY SEWER INSTALLATION.
5. COMPLETE FINAL RESTORATION.
6. REMOVE WORK ZONE TRAFFIC CONTROL MEASURES.
7. REMOVE TEMPORARY EROSION CONTROL DEVICES AFTER 70% GROWTH DENSITY HAS OCCURRED IN 100% OF RESTORATION AREAS. RESTORE DISTURBED DEVICES AND CLEAN OUT STORM WATER STRUCTURES. PERFORM FINAL CLEANING OF THE SITE.

GENERAL WATER MAIN CONSTRUCTION NOTES

1. WATER MAIN BURY DEPTH TO THE TOP OF THE PIPE SHALL BE 6.0' MINIMUM.
2. FINAL GRADE ON ALL VALVE BOXES LOCATED IN PAVEMENT SHALL BE 3/8" TO 1/2" BELOW FINISH GRADE. FINAL GRADE ON ALL VALVE BOXES LOCATED IN TURF AREAS SHALL BE 6" ABOVE FINISH GRADE.
3. PRIOR TO CONSTRUCTION, VERIFY LOCATION AND ELEVATION OF CONNECTION POINTS INCLUDING MAINS AND SERVICES. NOTIFY ENGINEER OF ANY DISCREPANCIES.
4. CONTRACTOR MUST FOLLOW CITY OF PEWAUKEE WATER MAIN INSTALLATION SEQUENCE OUTLINED IN THE CITY OF PEWAUKEE TECHNICAL STANDARDS AND IN SPECIFICATION 33 11 00.



BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
GENERAL NOTES
CITY OF PEWAUKEE
WAUKESHA COUNTY, WISCONSIN

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GN-01

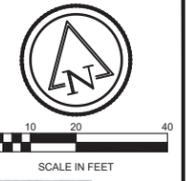
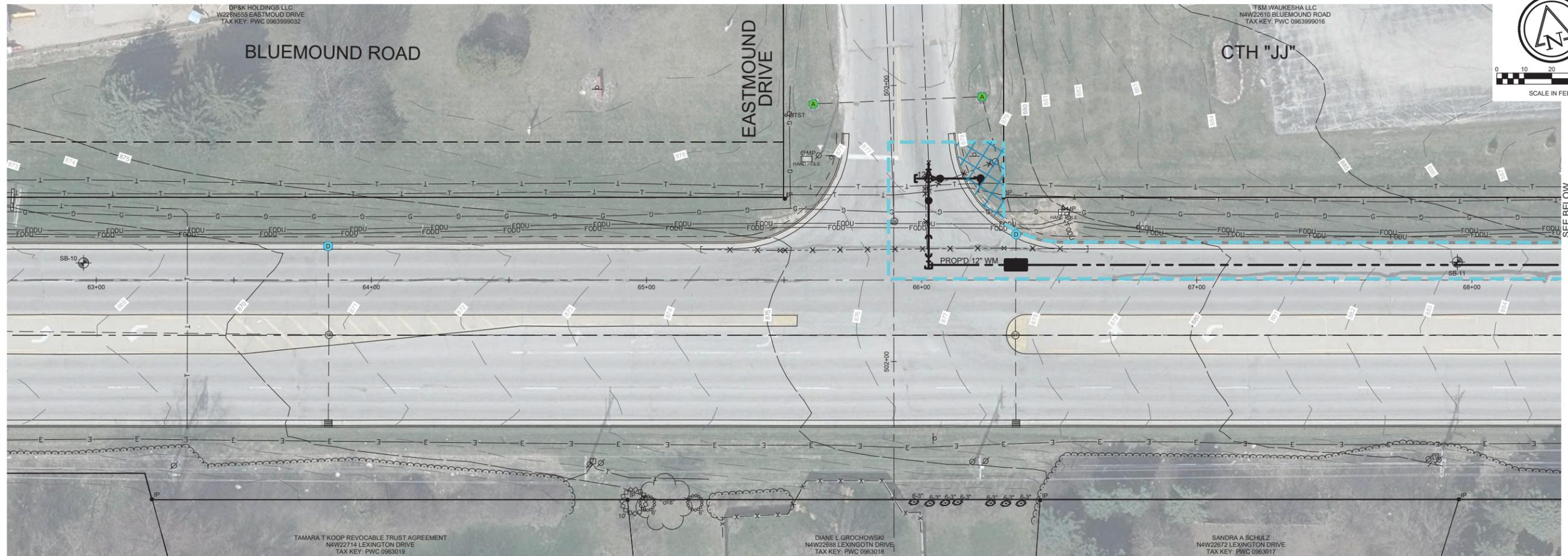
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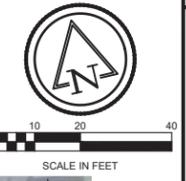
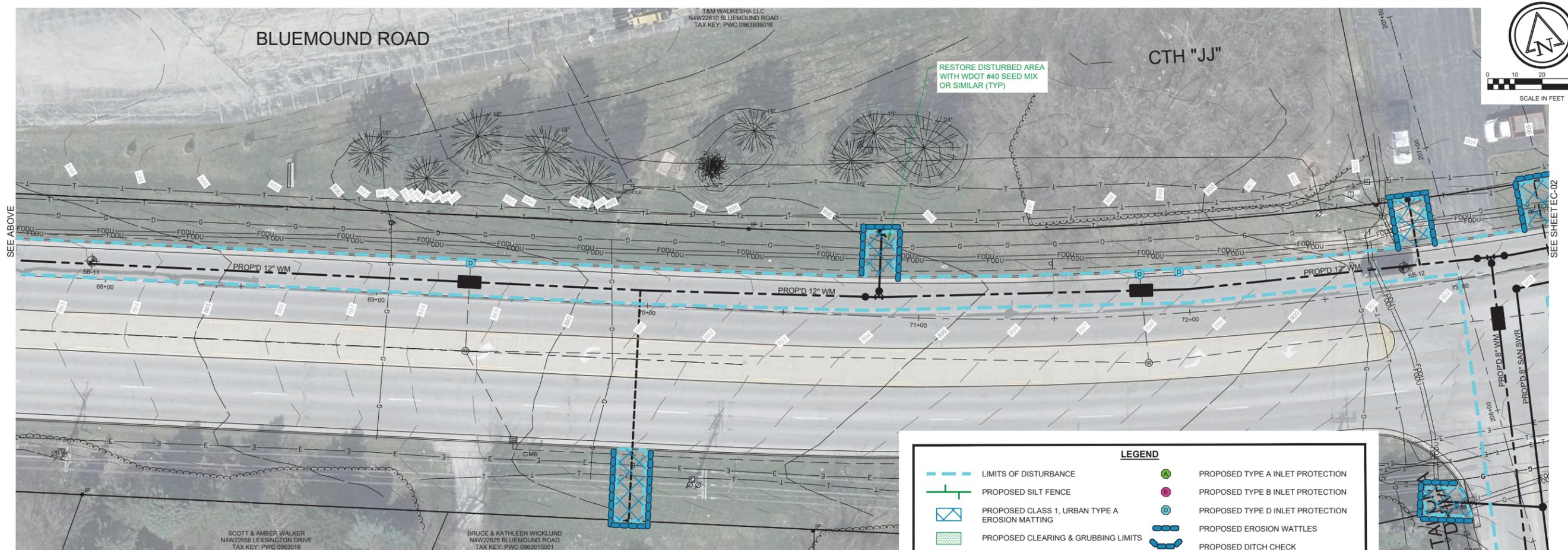
2025 - BLUEMOUND ROAD EROSION CONTROL - ZOPP, Blumound Road Erosion Control



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LEGEND

	LIMITS OF DISTURBANCE		PROPOSED TYPE A INLET PROTECTION
	PROPOSED SILT FENCE		PROPOSED TYPE B INLET PROTECTION
	PROPOSED CLASS 1, URBAN TYPE A EROSION MATTING		PROPOSED TYPE D INLET PROTECTION
	PROPOSED CLEARING & GRUBBING LIMITS		PROPOSED EROSION WATTLES
			PROPOSED DITCH CHECK

SEE ABOVE

SEE SHEET EC-02

SEE SHEET EC-03

BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
BLUEMOUND ROAD/CTH "JJ" EROSION CONTROL PLAN
 CITY OF PEWAUKEE
 WAUKESHA COUNTY, WISCONSIN

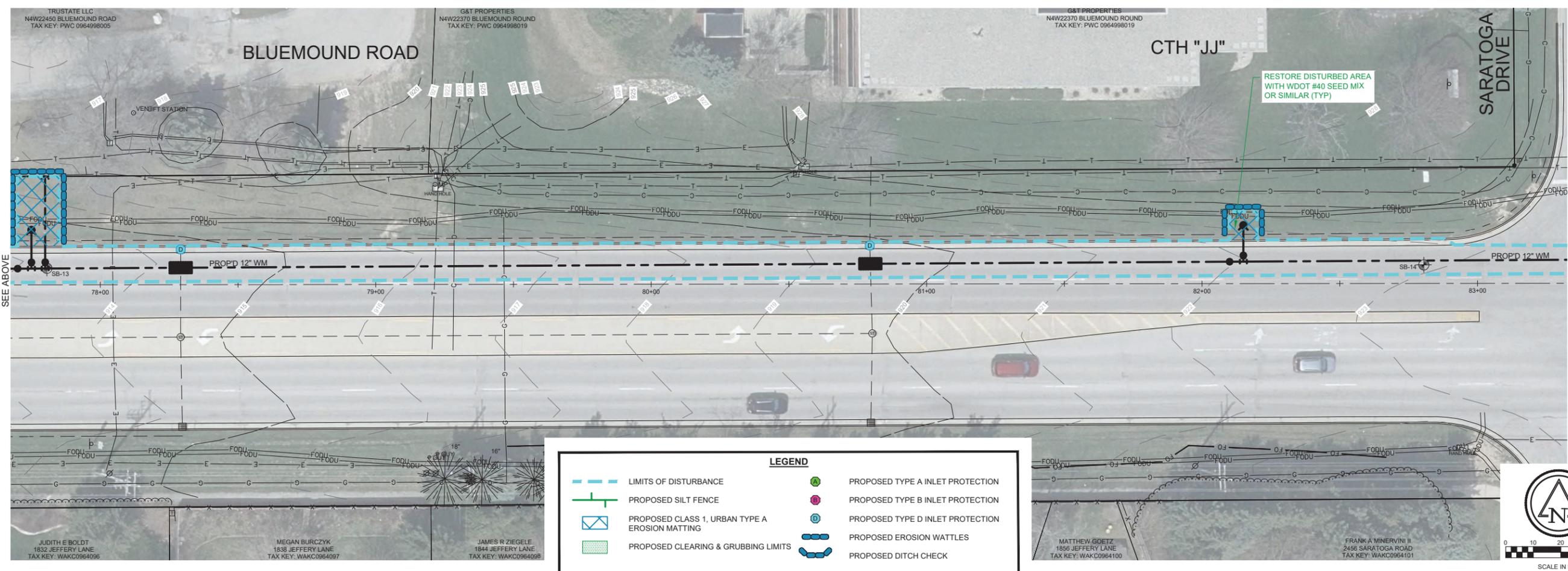
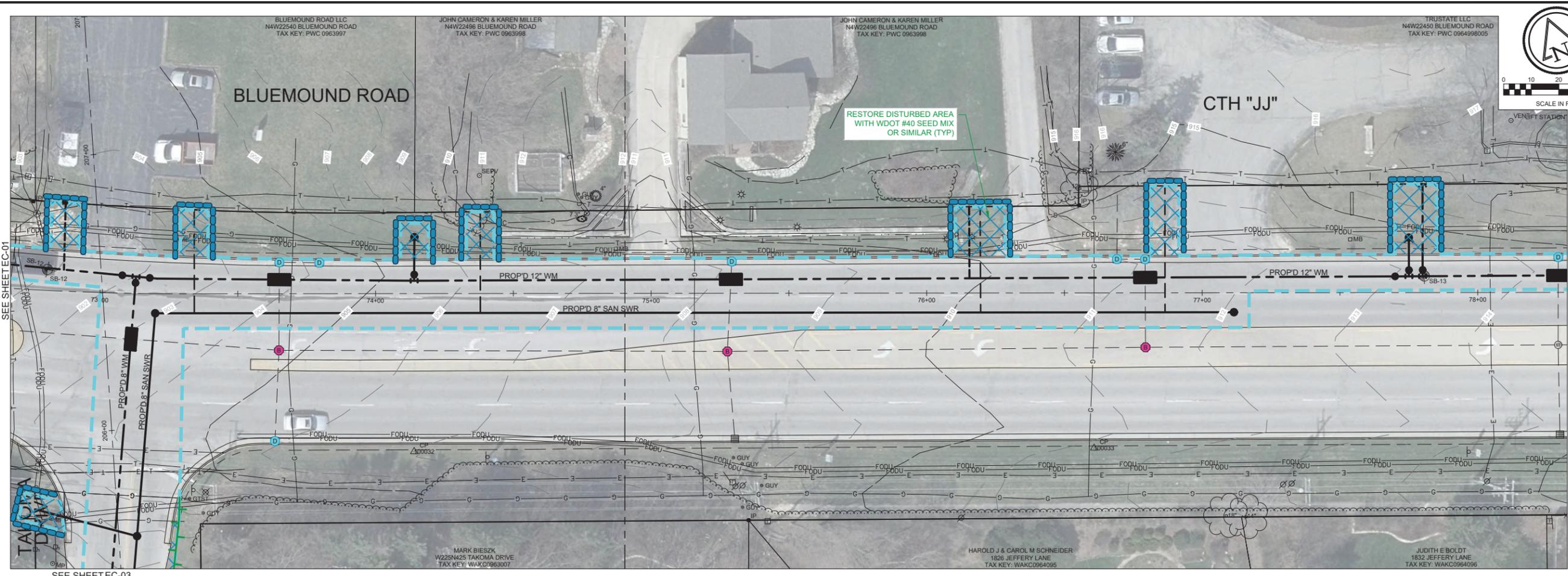
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EC-01

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2025 - BLUEMOUND ROAD EROSION CONTROL - ZIPP - BlueMound Road Erosion Control



LEGEND

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	PROPOSED SILT FENCE		PROPOSED TYPE B INLET PROTECTION
	PROPOSED CLASS 1, URBAN TYPE A EROSION MATTING		PROPOSED TYPE D INLET PROTECTION
	PROPOSED CLEARING & GRUBBING LIMITS		PROPOSED EROSION WATTLES
			PROPOSED DITCH CHECK

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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
BLUEMOUND ROAD/CTH "JJ" EROSION CONTROL PLAN
 CITY OF PEWAUKEE
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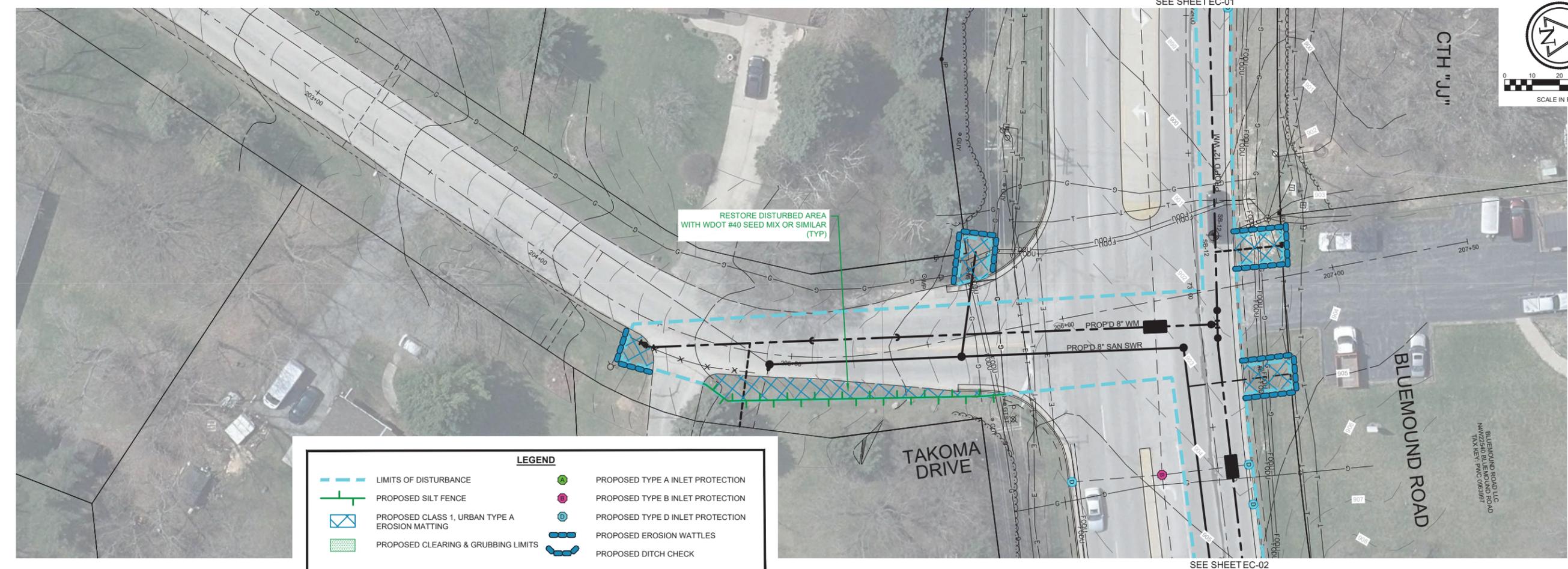
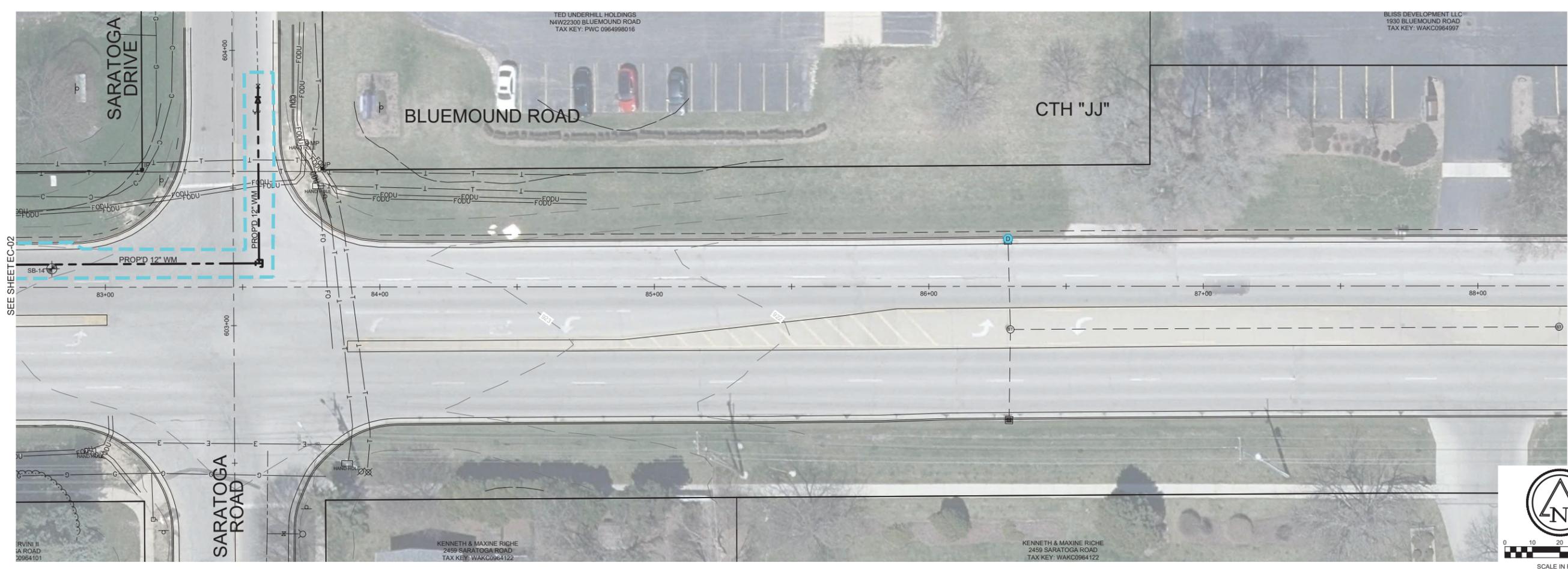
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	PROPOSED CLASS 1, URBAN TYPE A EROSION MATTING		PROPOSED TYPE D INLET PROTECTION
	PROPOSED CLEARING & GRUBBING LIMITS		PROPOSED EROSION WATTLES
			PROPOSED DITCH CHECK

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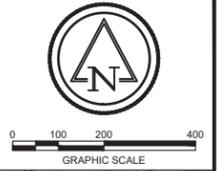
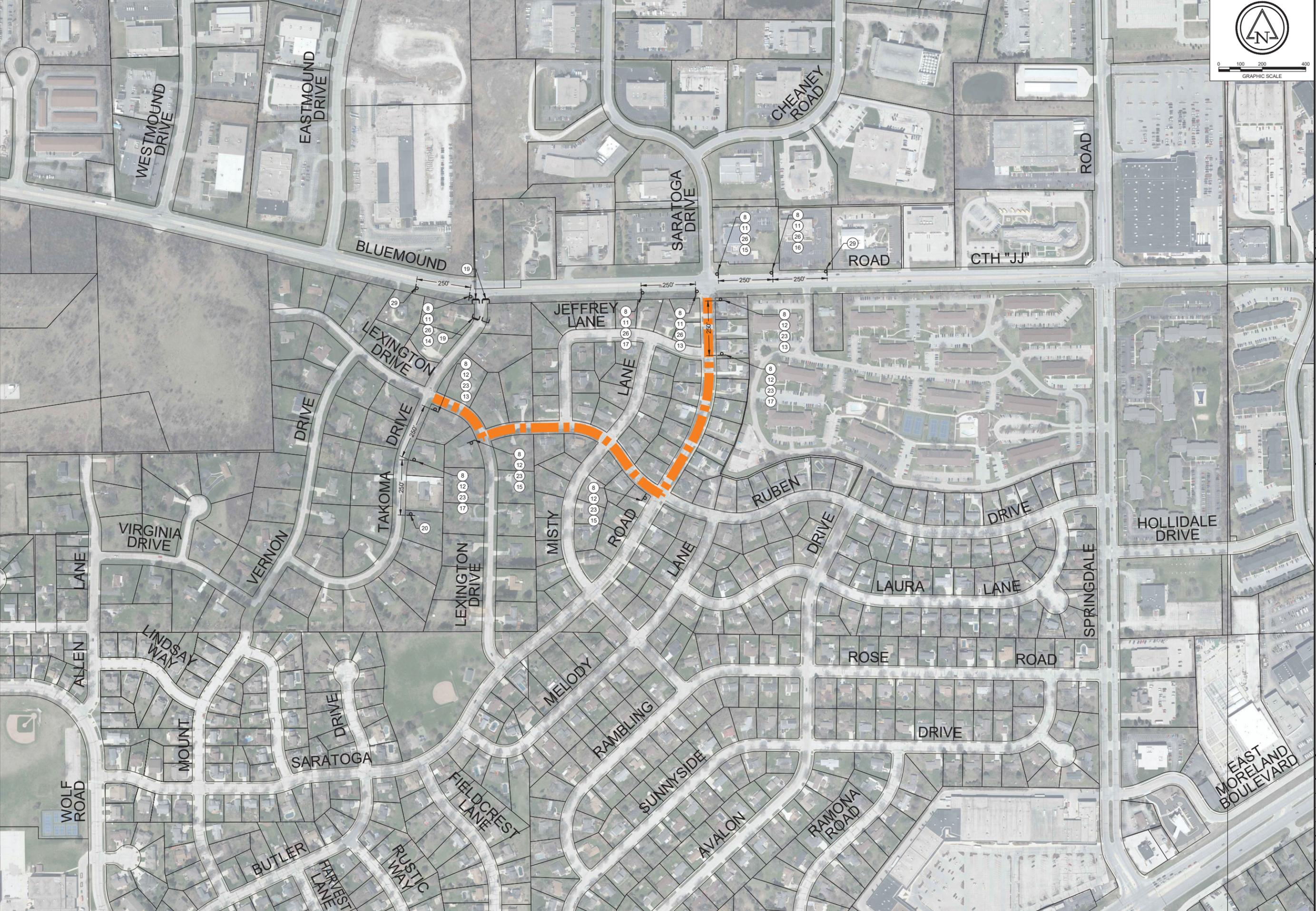
BLUE MOUND ROAD WATER & SEWER - TAKOMA EXTENSION
BLUE MOUND ROAD/CTH "JJ" & TAKOMA DRIVE EROSION CONTROL PLAN
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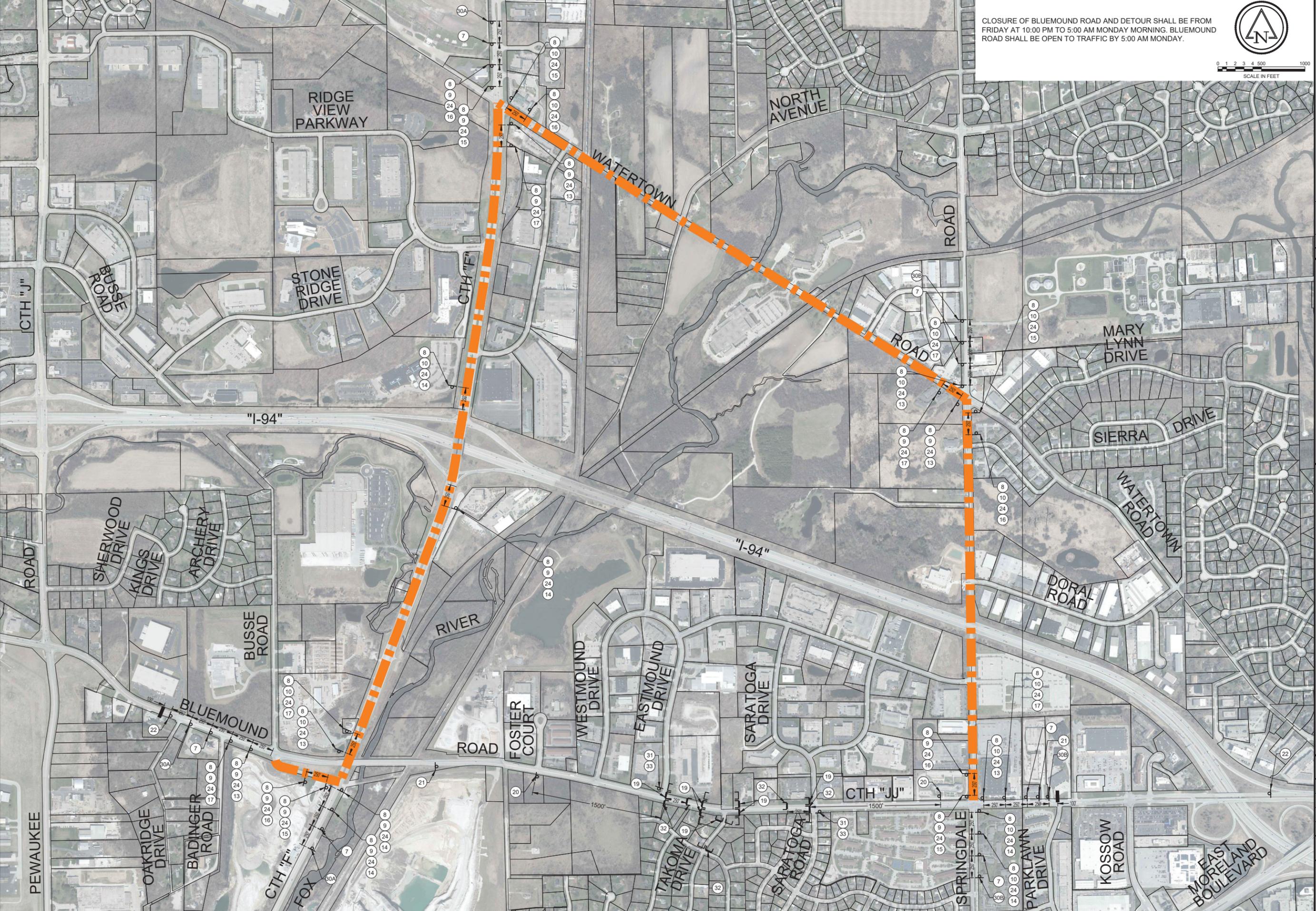
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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
 STAGE 1A/1B TRAFFIC CONTROL PLAN - TAKOMA DRIVE DETOUR ROUTE
 CITY OF PEWAUKEE
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CLOSURE OF BLUEMOUND ROAD AND DETOUR SHALL BE FROM FRIDAY AT 10:00 PM TO 5:00 AM MONDAY MORNING. BLUEMOUND ROAD SHALL BE OPEN TO TRAFFIC BY 5:00 AM MONDAY.

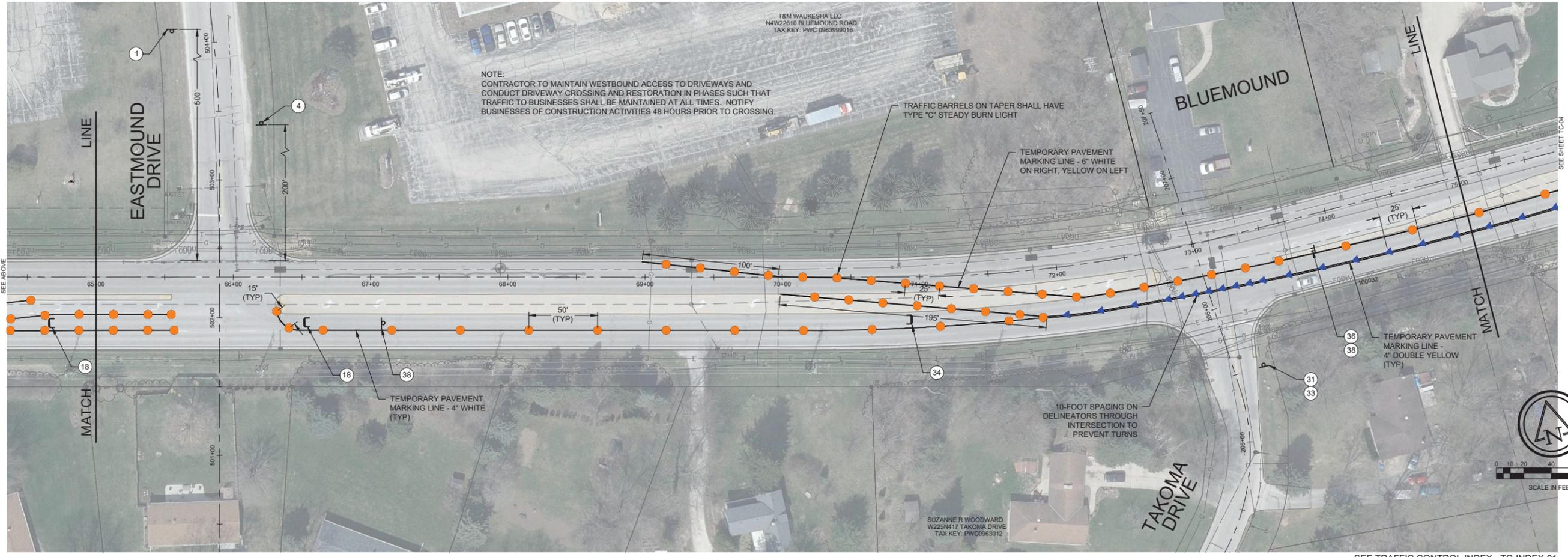
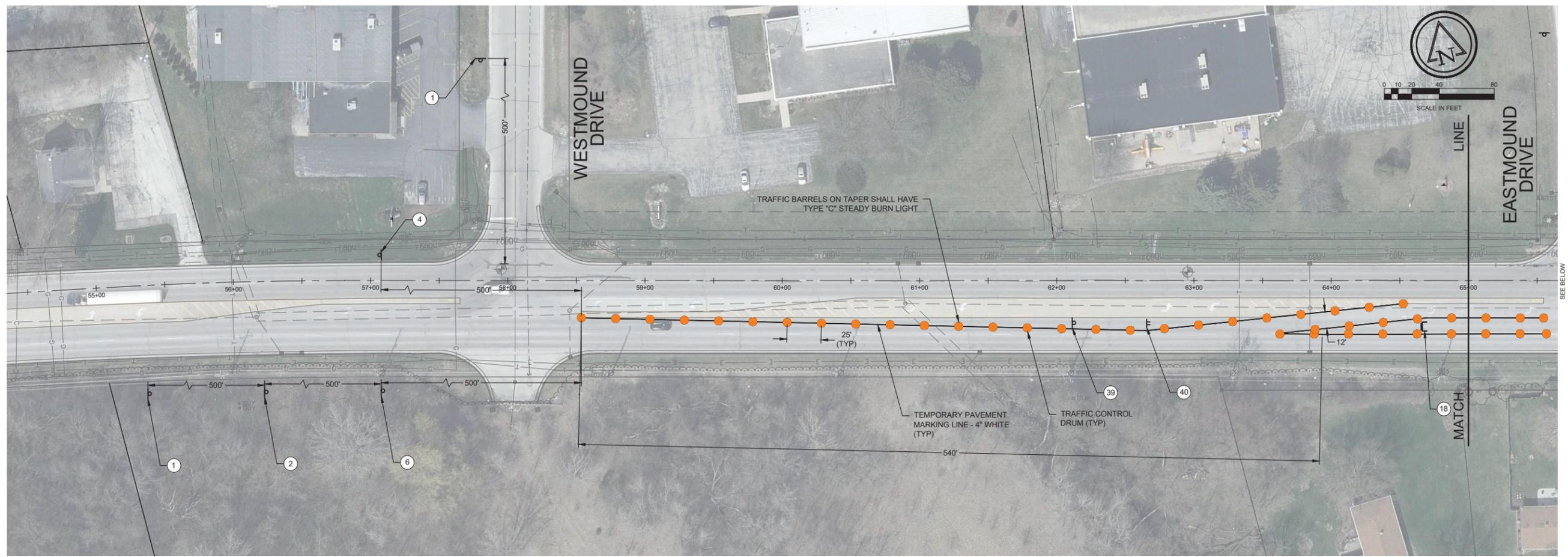
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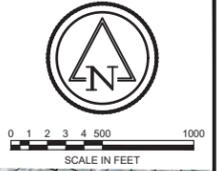
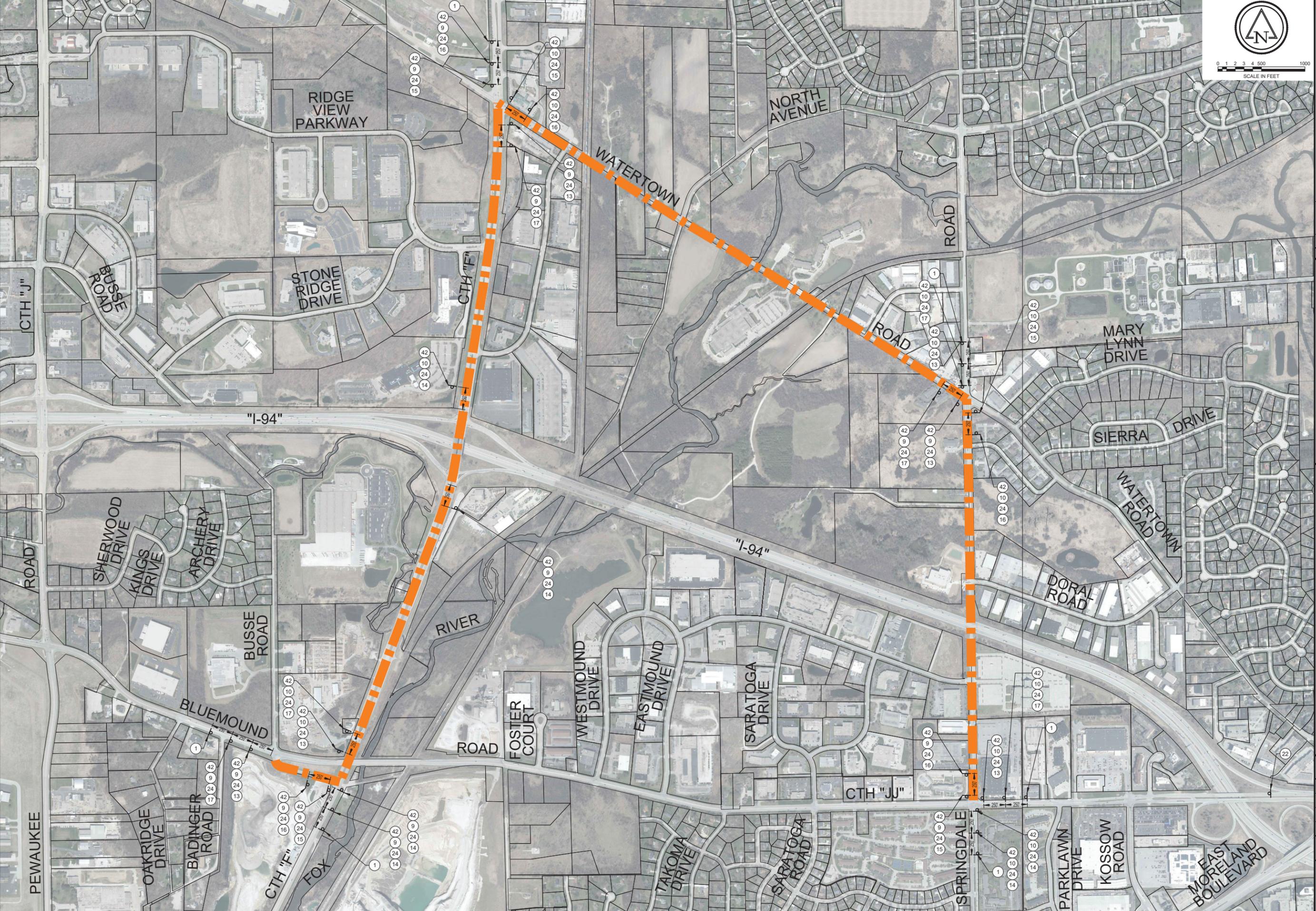
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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
STAGE 1C TRAFFIC CONTROL PLAN - BLUEMOUND ROAD
 CITY OF PEWAUKEE
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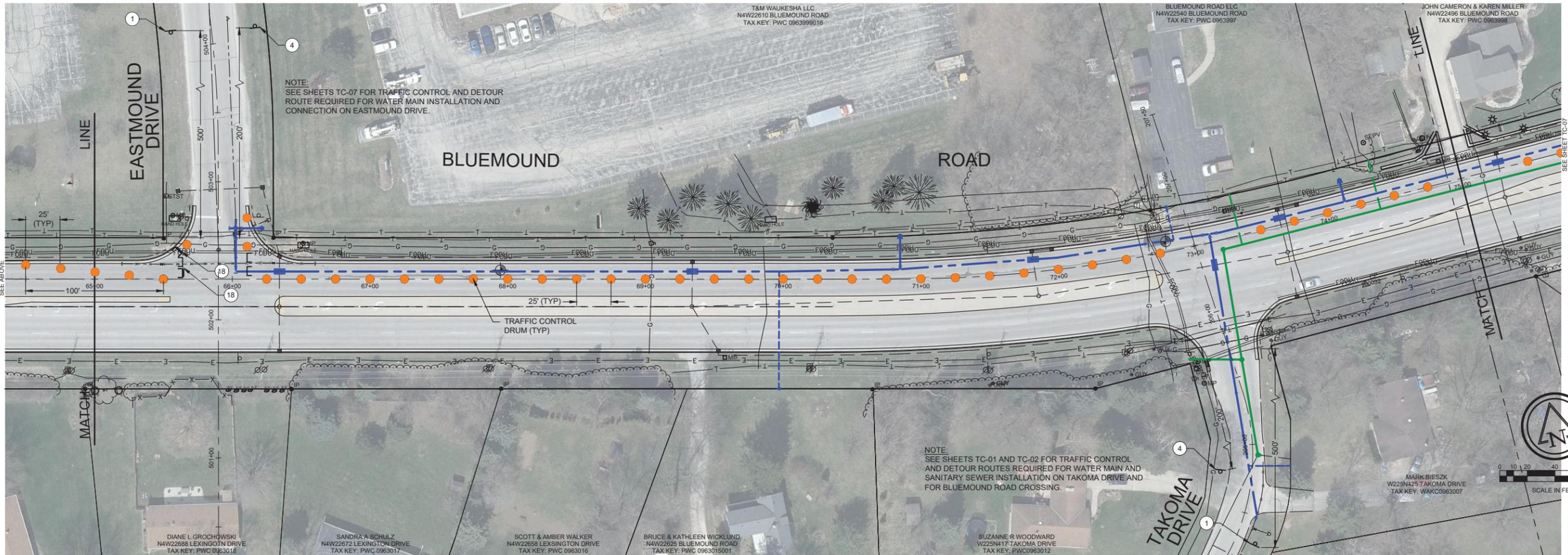
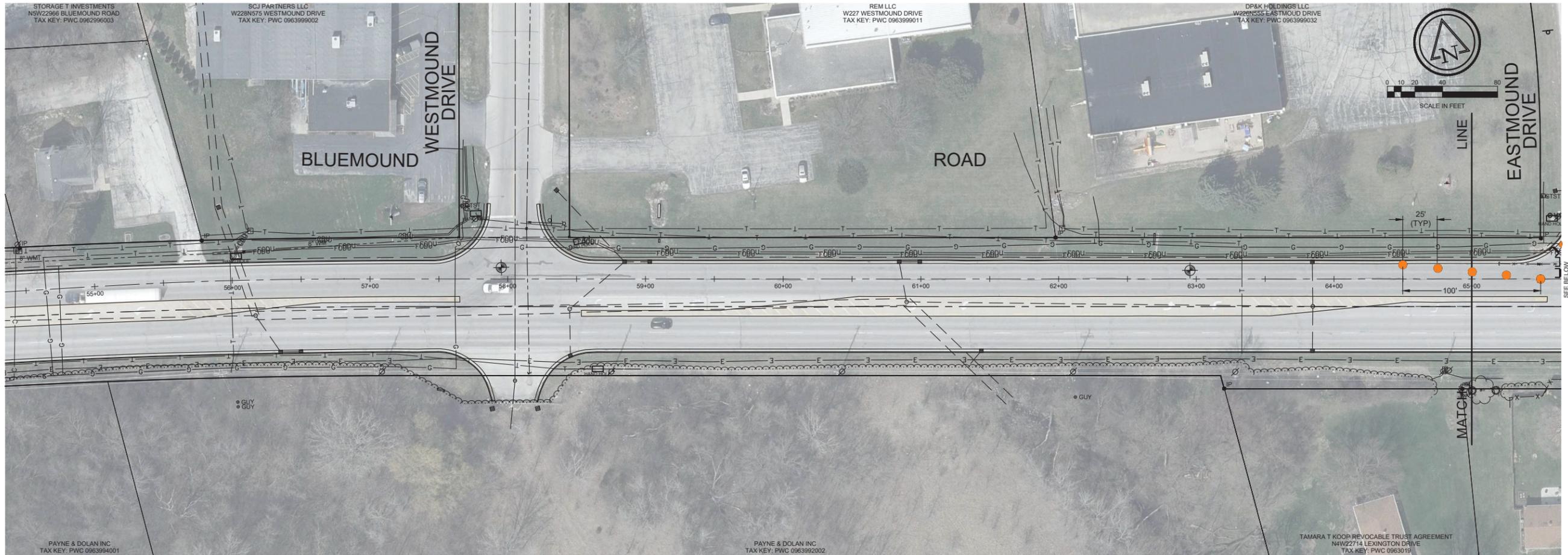
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<p>BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION</p> <p>STAGE 1C TRAFFIC CONTROL PLAN - BLUEMOUND ROAD ALTERNATE ROUTE</p> <p>CITY OF PEWAUKEE WAUKESHA COUNTY, WISCONSIN</p>		

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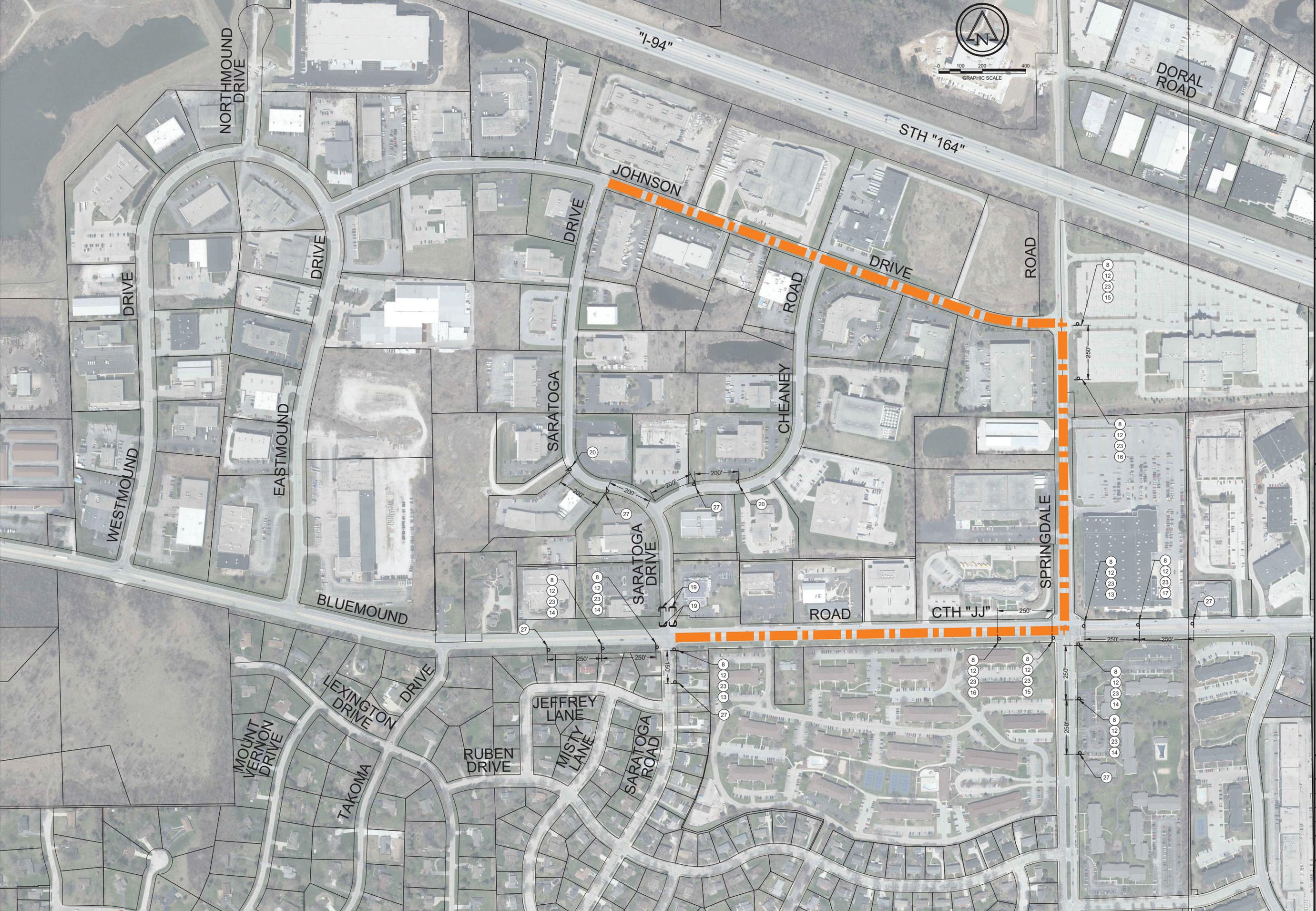
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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
STAGE 2A TRAFFIC CONTROL PLAN - BLUEMOUND ROAD
 CITY OF PEWAUKEE
 WAUKESHA COUNTY, WISCONSIN

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TC-06

SEE TRAFFIC CONTROL INDEX - TC-INDEX-01

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A	8	7	6	5	4	3	2	1
REVISIONS								

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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
STAGE 2C TRAFFIC CONTROL PLAN - SARATOGA DRIVE DETOUR ROUTE
 CITY OF PEWAUKEE
 WAUKESHA COUNTY, WISCONSIN

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 DATE: MARCH 2025

FILE NO.
26-10144.200

BID SET
 SHEET NO.
TC-09

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BID SET

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC
- WORK AREA
- CONNECTED ARROW BOARD
- WZ START LOCATION MARKER
- WZ END LOCATION MARKER

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN

PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

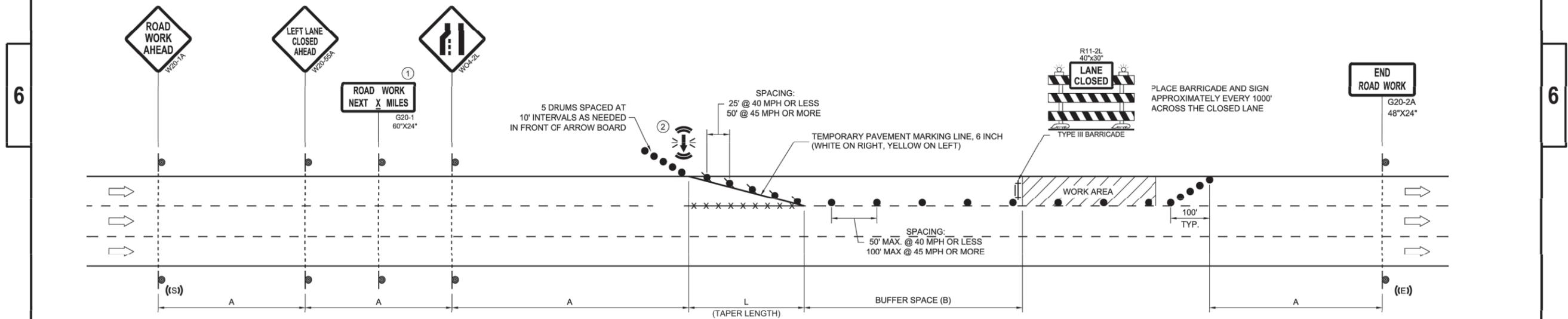
CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ② WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2024 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

SDD 15D20-09a

SDD 15D20-09a

9	8	7	6	5	4	3	2	1
A	B	C	D	E	F	G	H	I

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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
TRAFFIC CONTROL DETAILS
CITY OF PEWAUKEE
WAUKESHA COUNTY, WISCONSIN

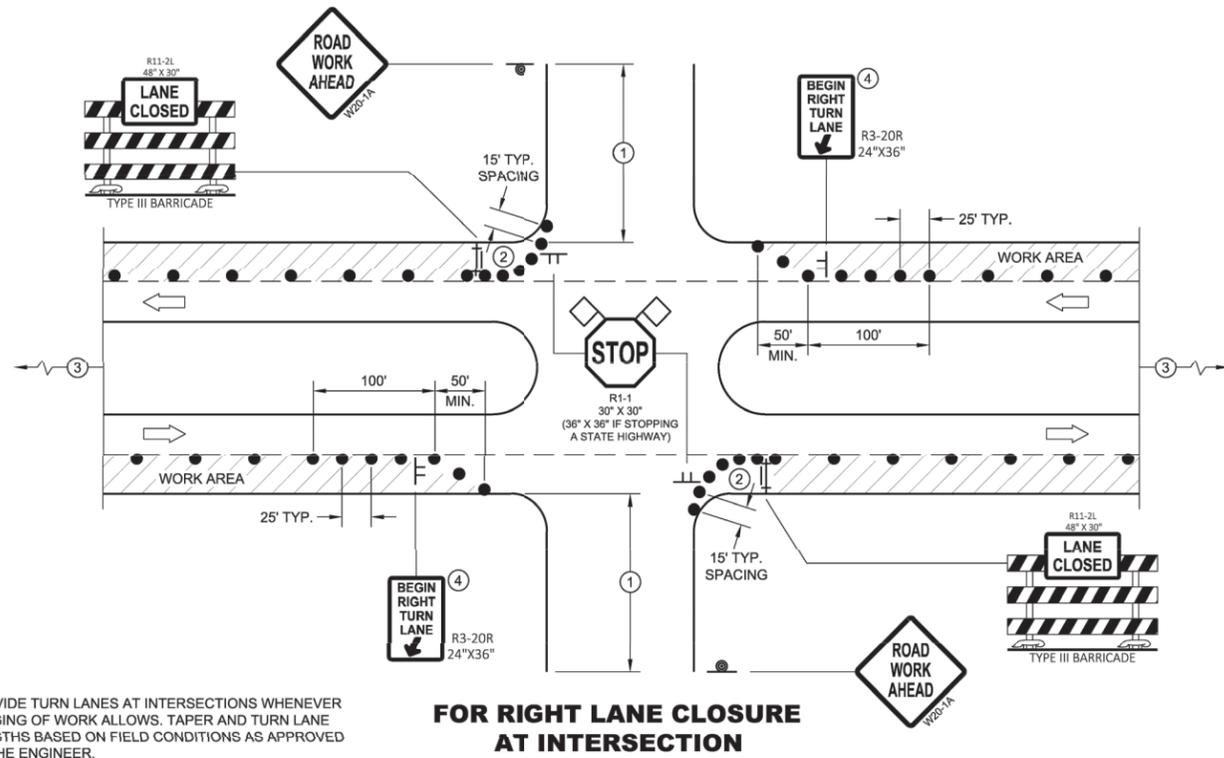
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APPROVED
November 2024 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

SHEET NO. **TC-11**



SDD 15D21-a Traffic Control, Intersection Within Single Right Lane Closure



GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

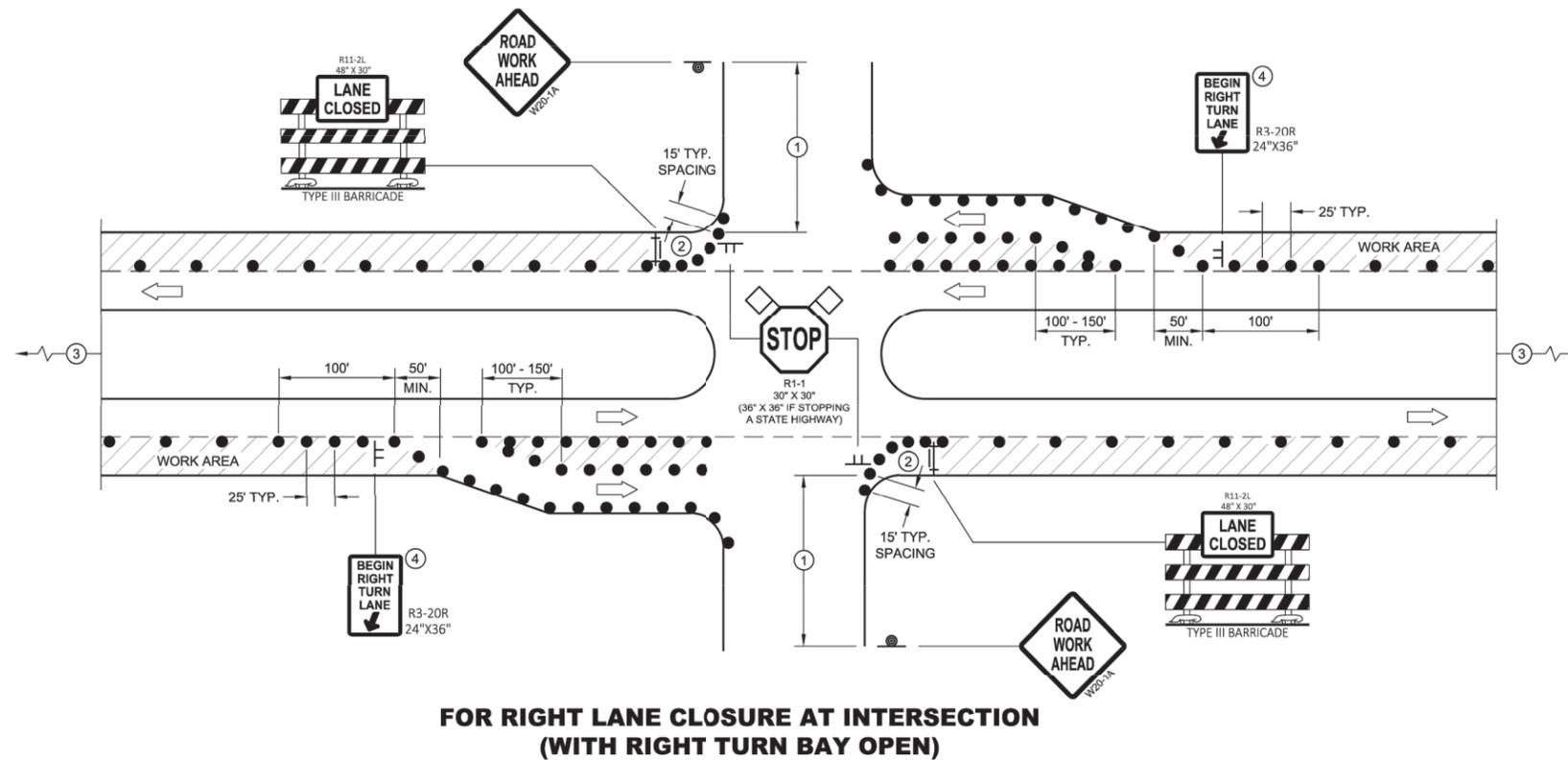
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

- ① 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.
- ② ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
- ④ MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.



LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE RIGHT LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15D21 - 07a

SDD 15D21 - 07a

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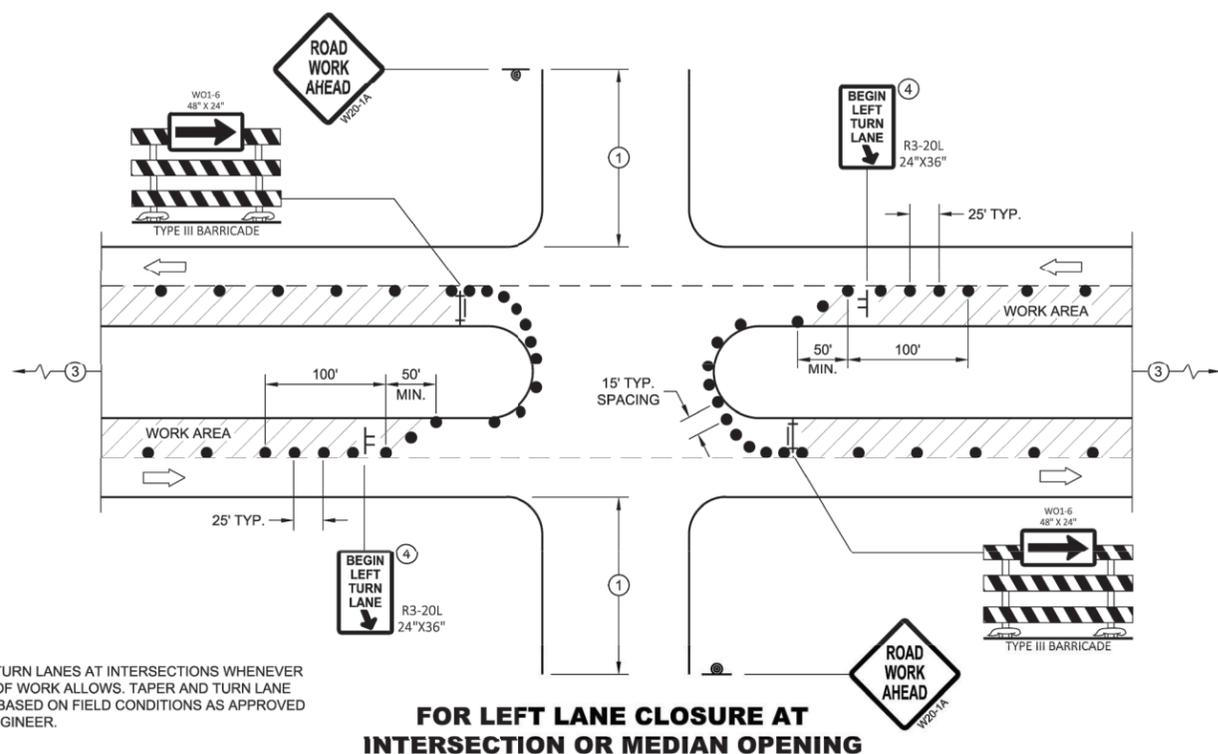
BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
 TRAFFIC CONTROL DETAILS
 CITY OF PEWAUKEE
 WAUKESHA COUNTY, WISCONSIN

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BID SET
 SHEET NO. TC-12



SDD 15D21-b Traffic Control, Intersection Within Single Left Lane Closure



GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

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THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

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SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

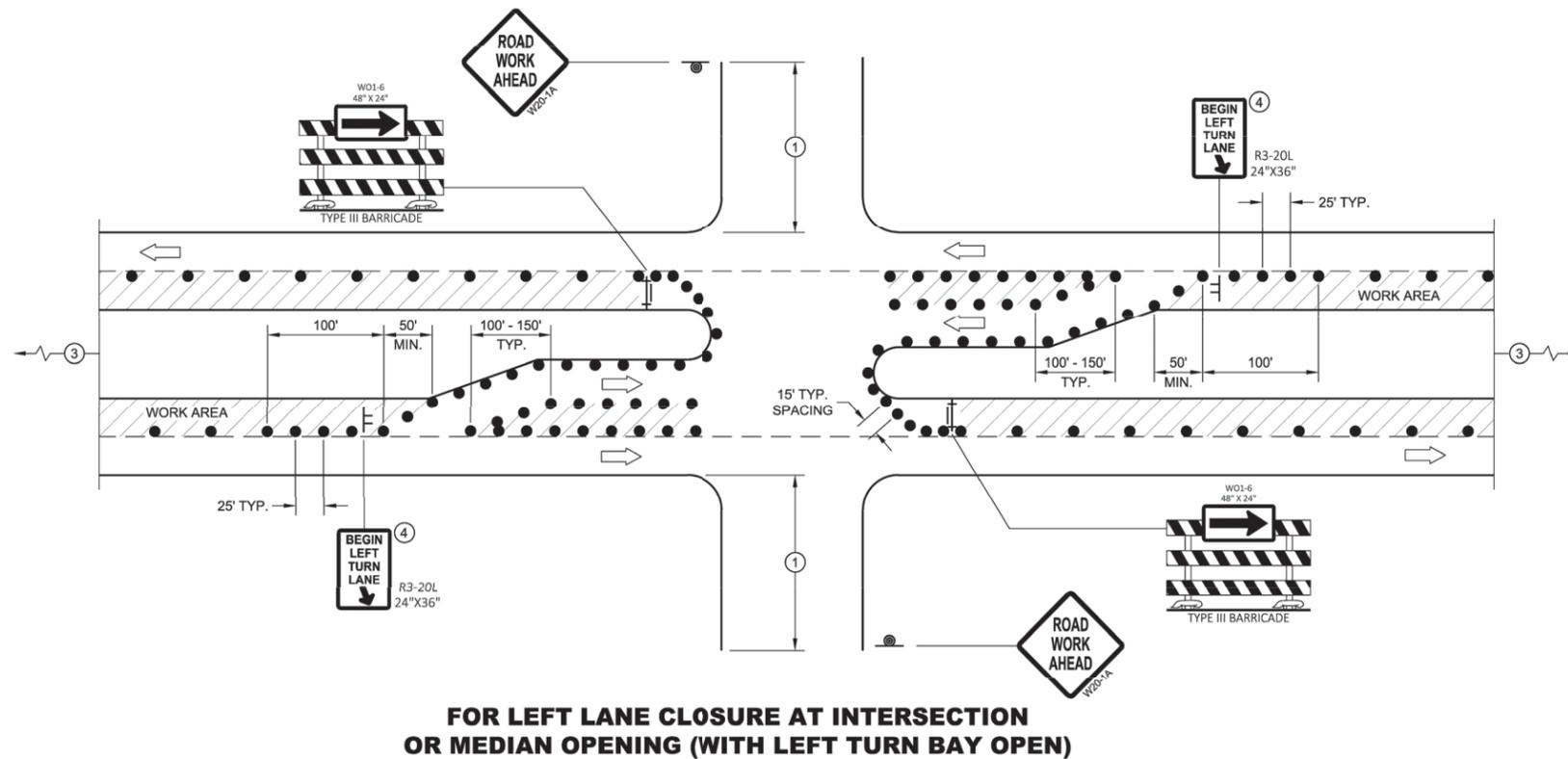
SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS. BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

- ① 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.
- ② ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
- ④ MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA



TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LEFT LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

6

SDD 15D21 - 07b

6

SDD 15D21 - 07b

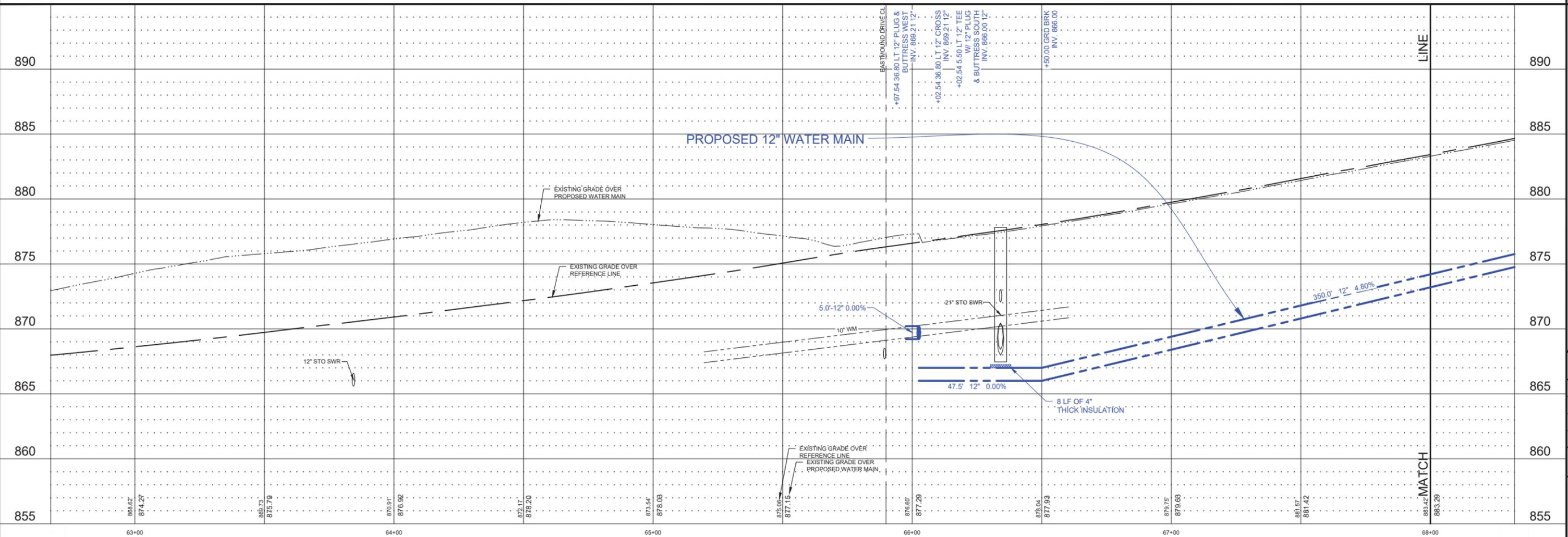
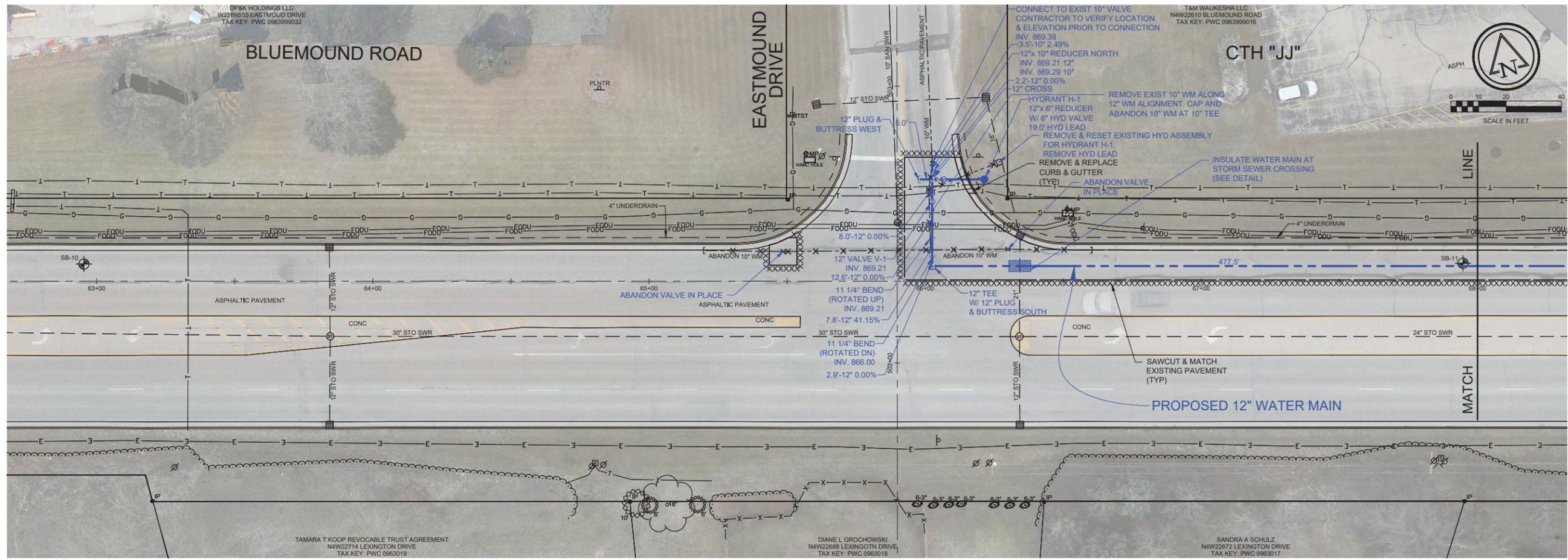
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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
TRAFFIC CONTROL DETAILS
CITY OF PEWAUKEE
WAUKESHA COUNTY, WISCONSIN

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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
PROPOSED WATER MAIN PLAN
 IN: BLUEMOUND ROAD/CTH "JJ"
 FROM: 300' WEST OF EASTMOUND DRIVE
 TO: 200' EAST OF EASTMOUND DRIVE
 CITY OF PEWAUKEE
 WAUKESHA COUNTY, WISCONSIN

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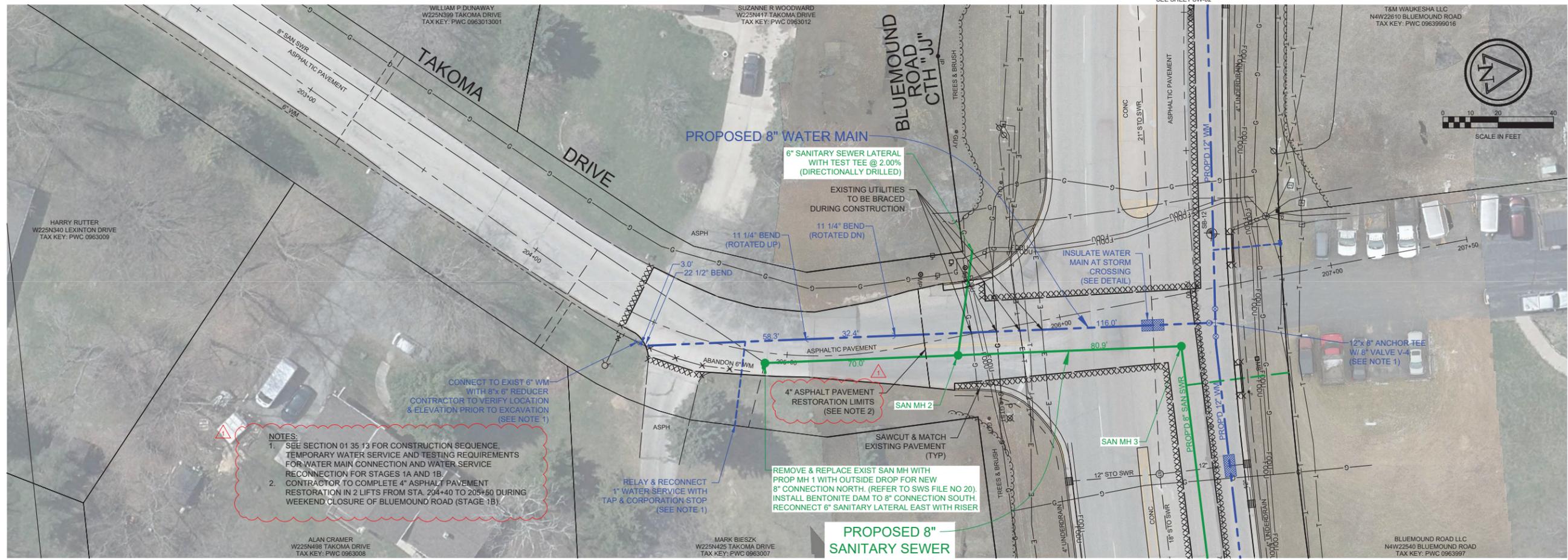
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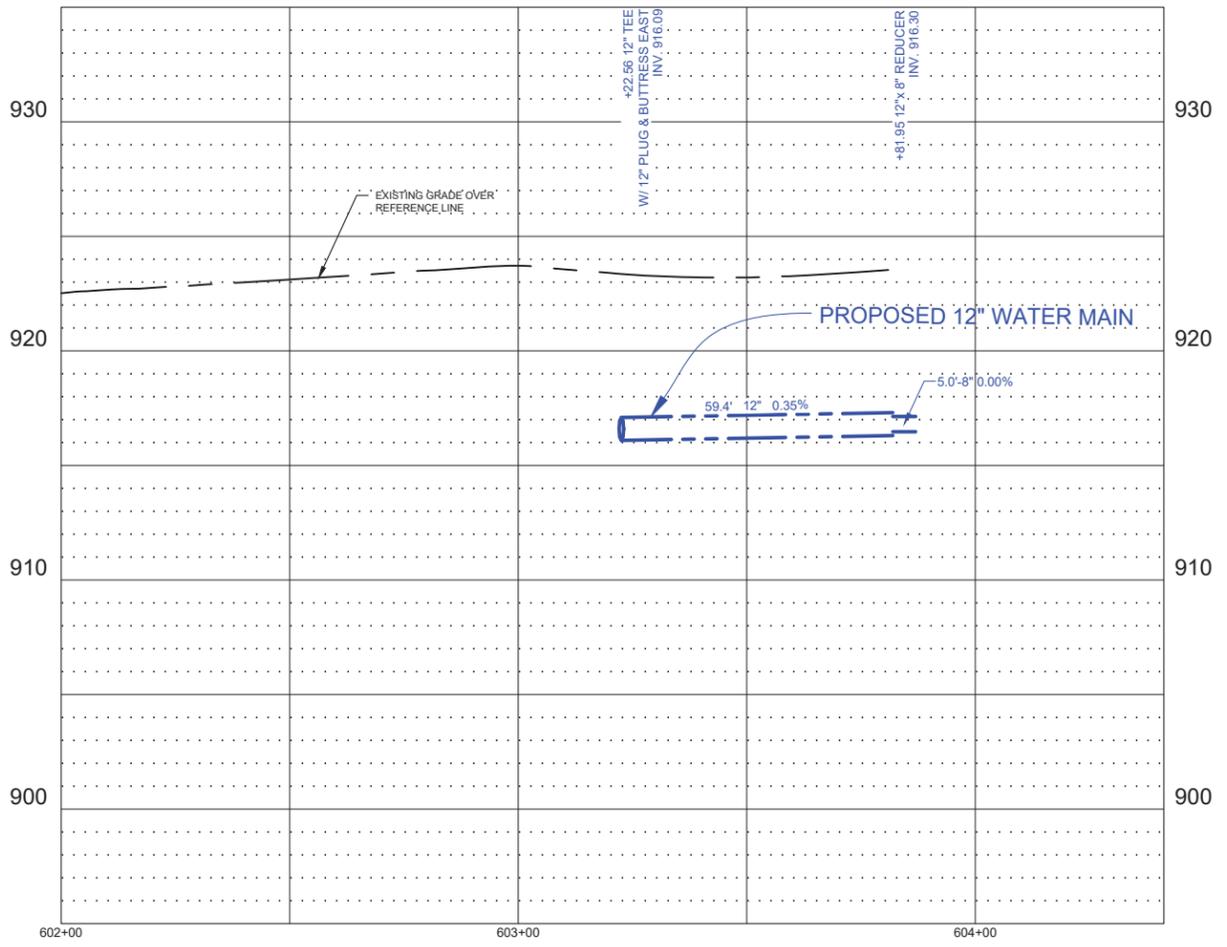
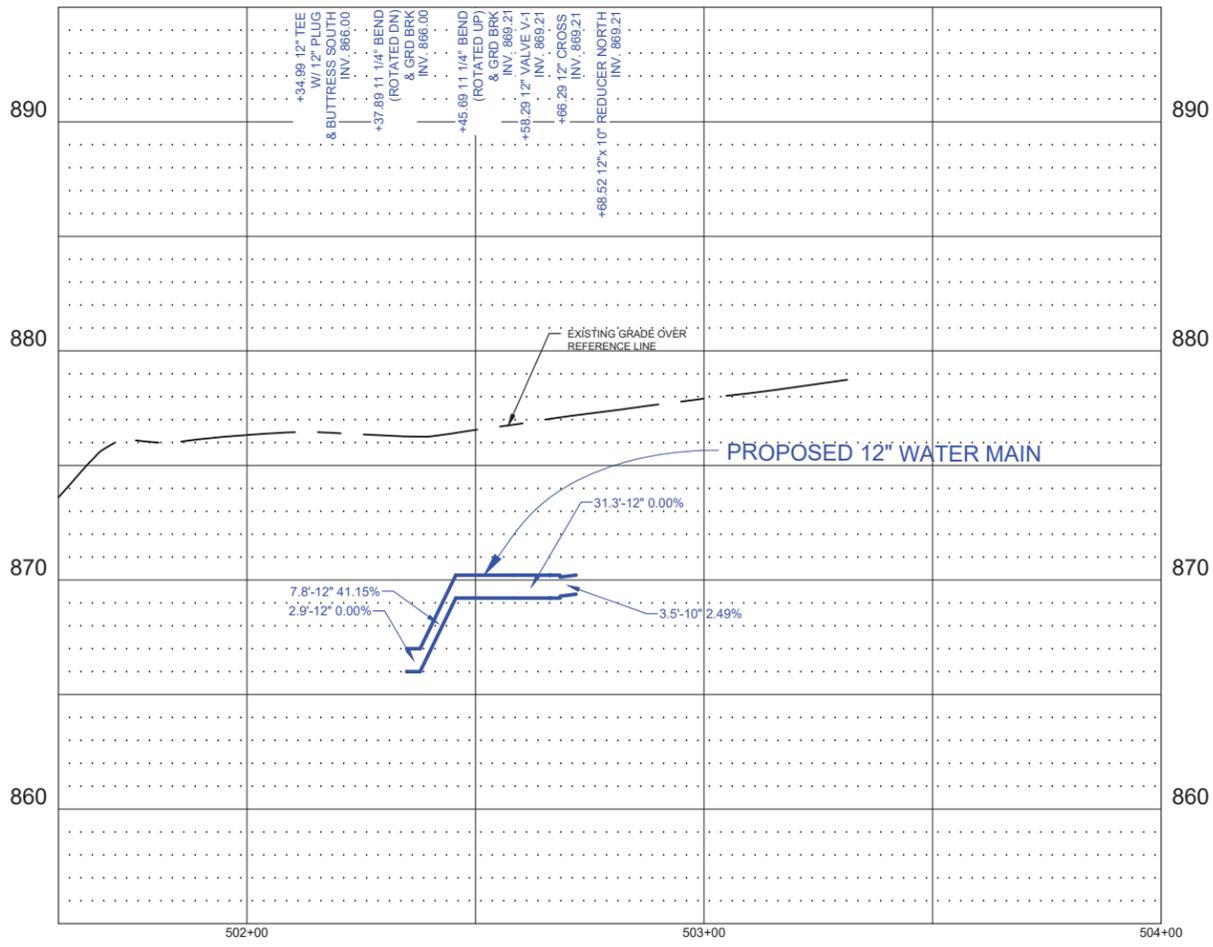
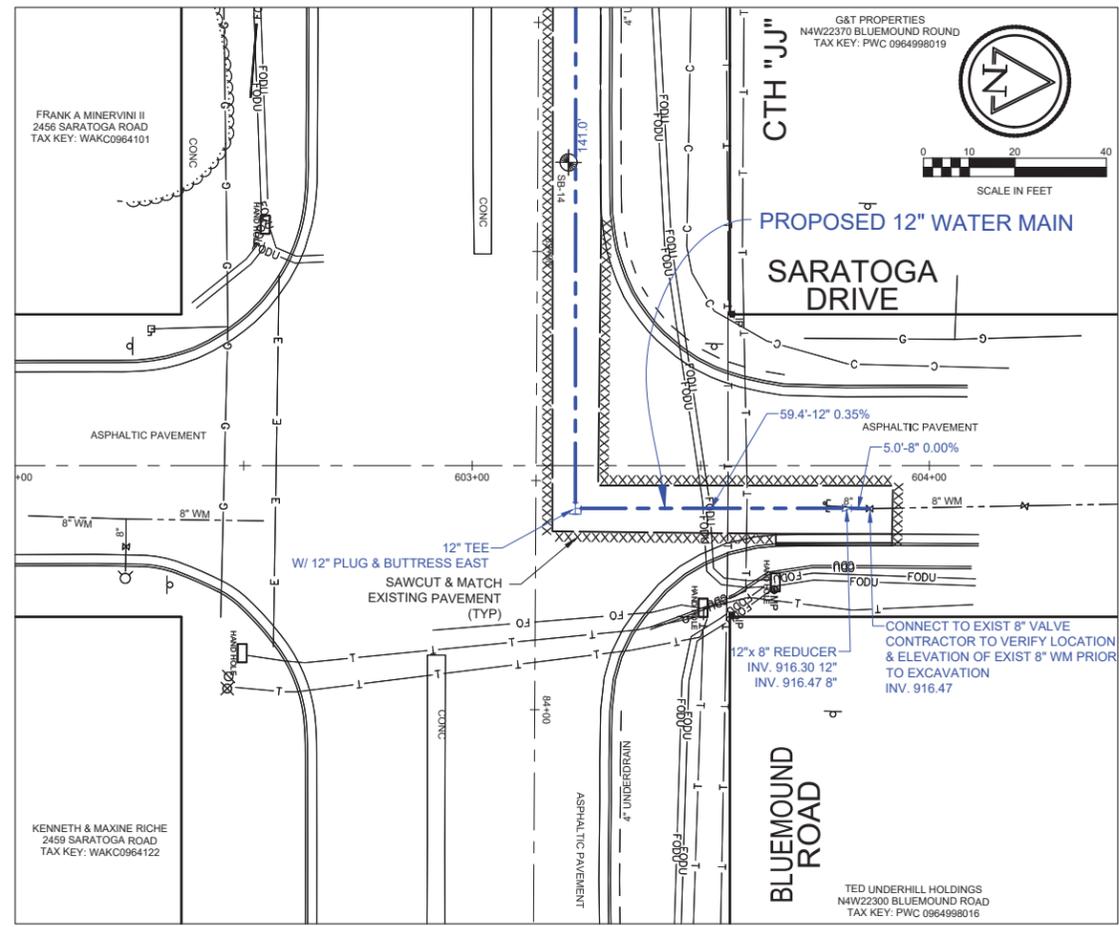
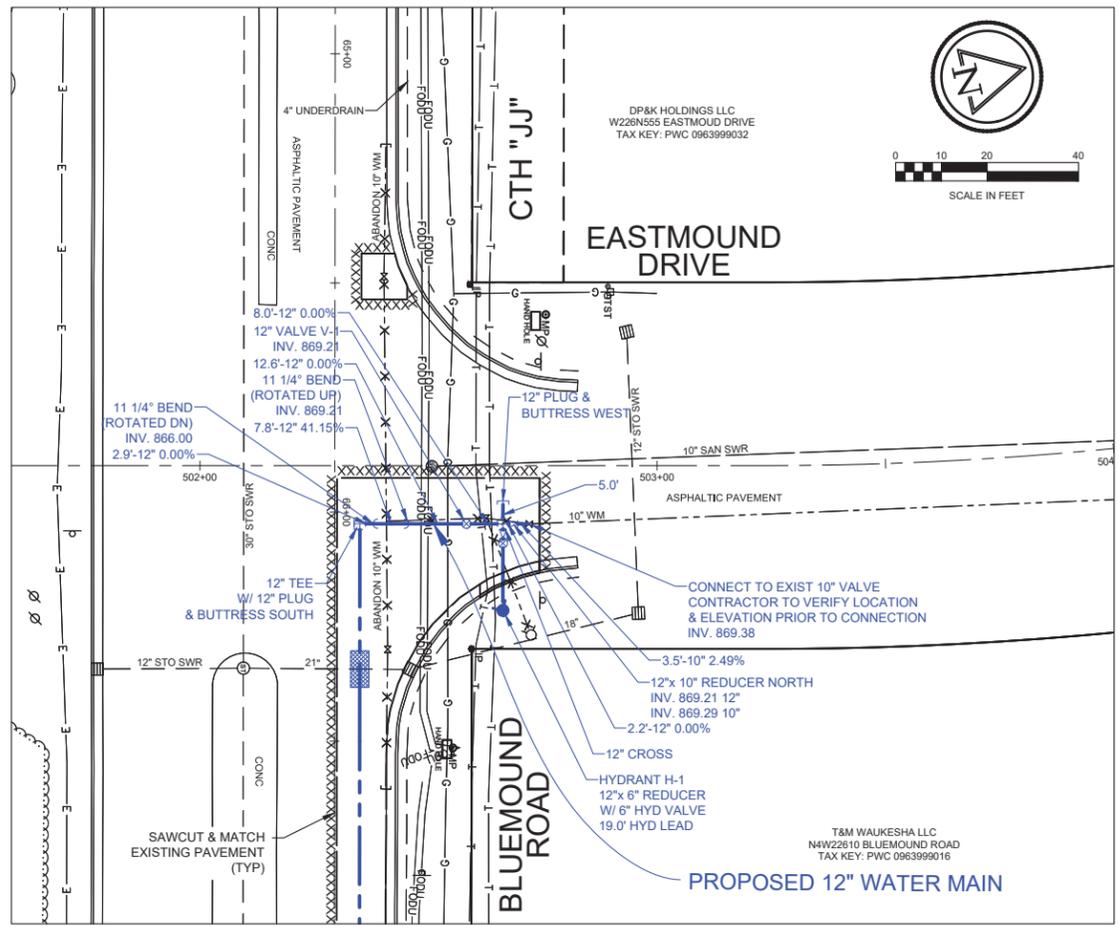
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BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
WATER MAIN CONNECTIONS - EASTMOUND DRIVE & SARATOGA DRIVE
 CITY OF PEWAUKEE
 WAUKESHA COUNTY, WISCONSIN

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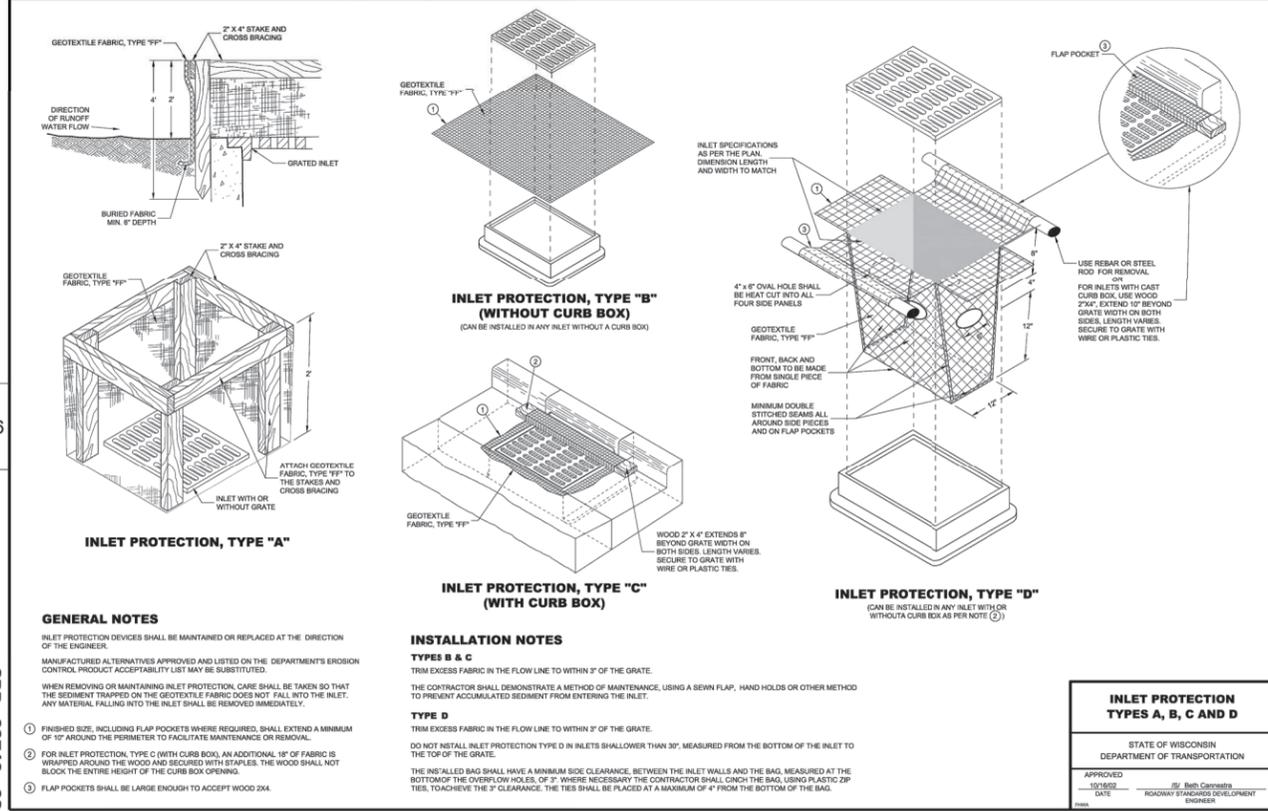
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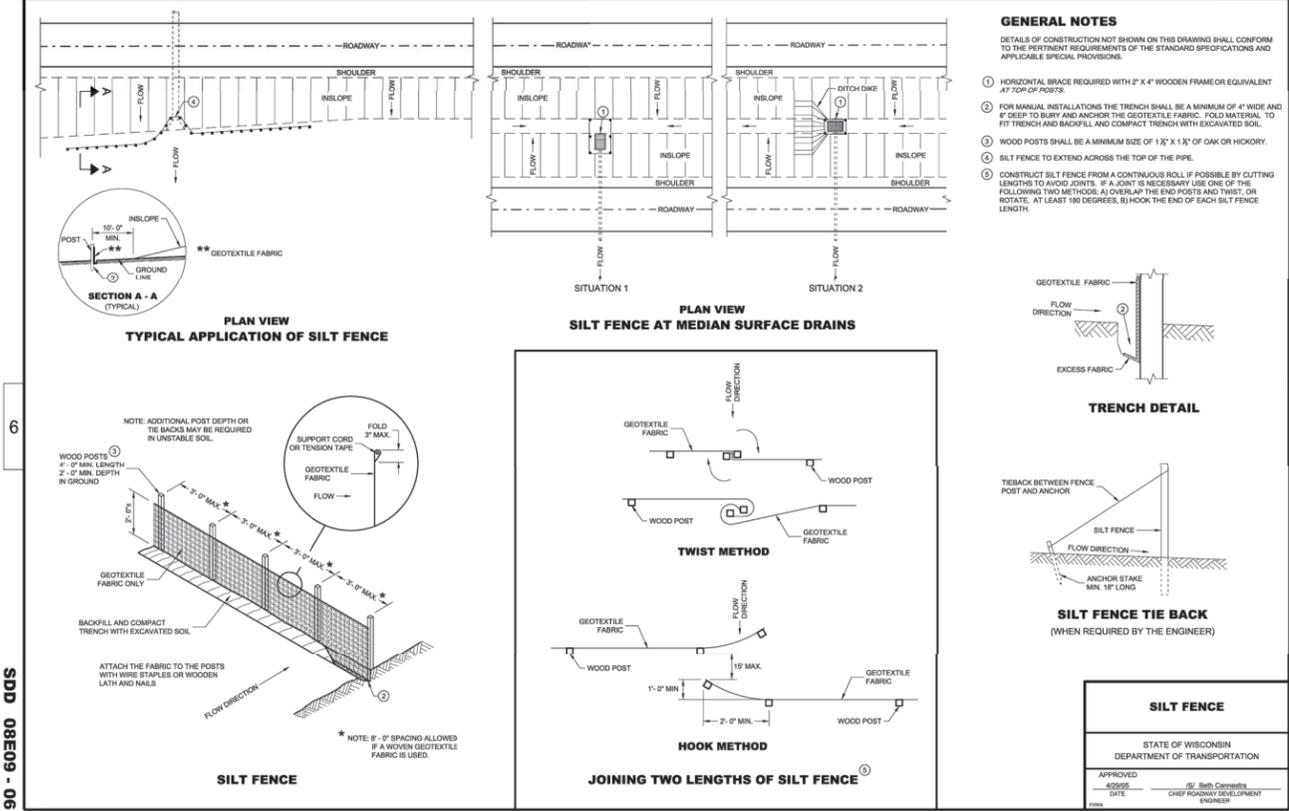
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SDD 08E10 Inlet Protection, Types A, B, C and D

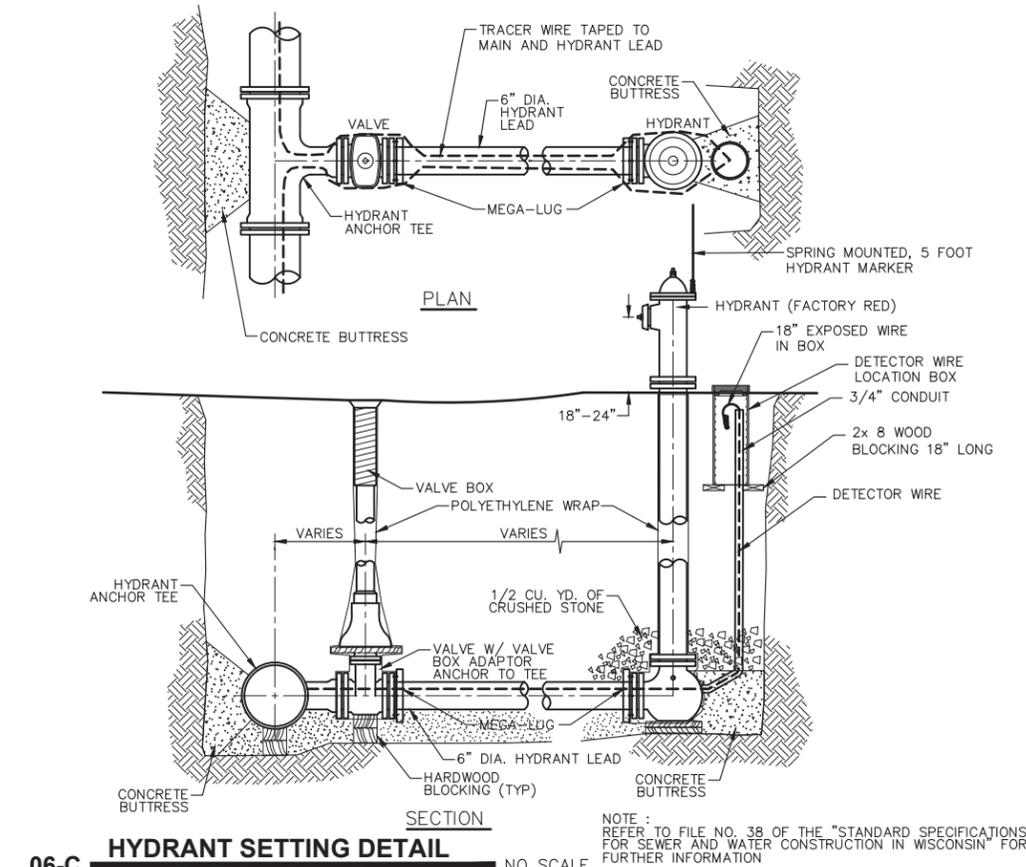


INLET PROTECTION TYPES A, B, C AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02	BY: Ruth Carver ROADWAY DEVELOPMENT ENGINEER

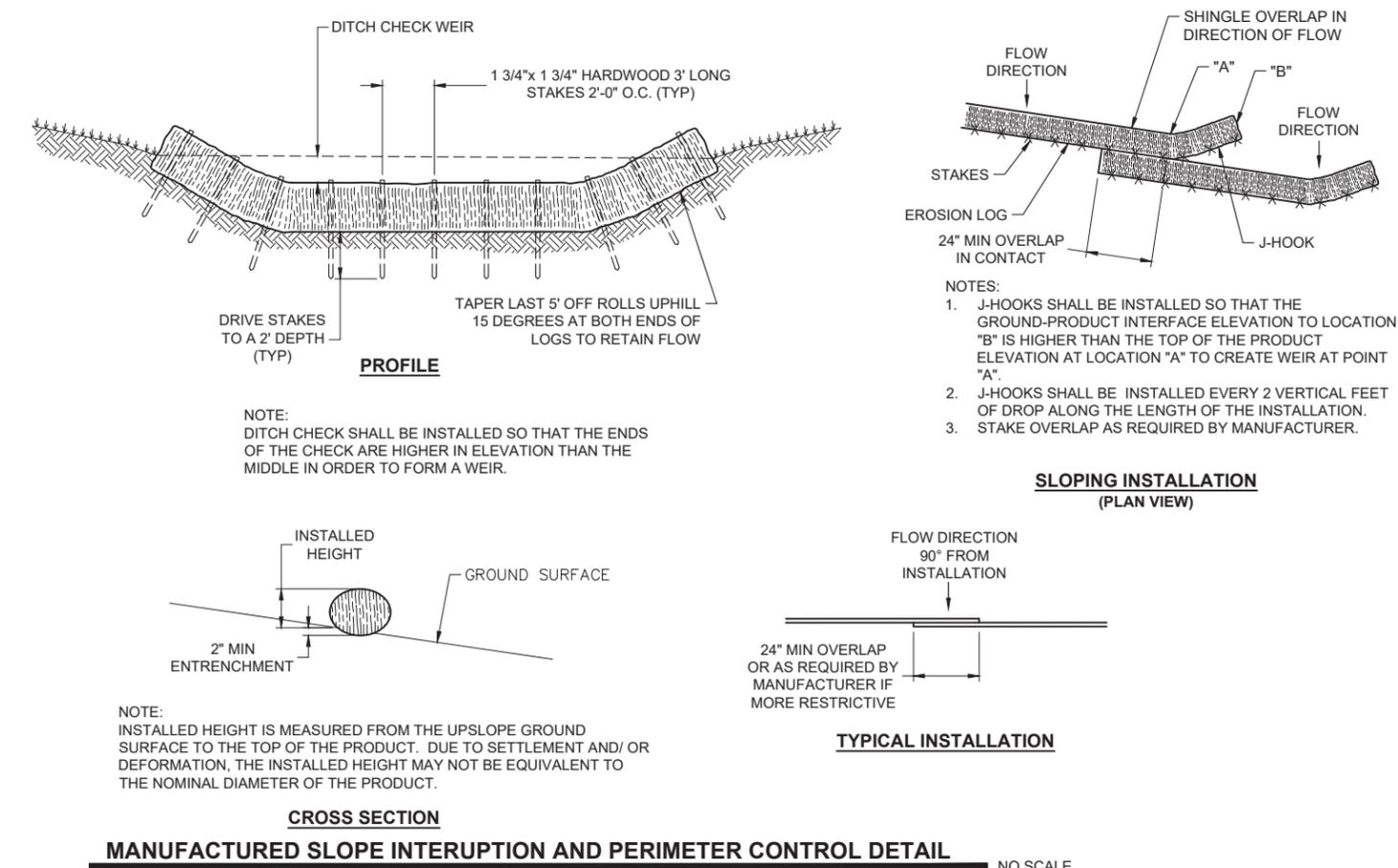
SDD 08E09 Silt Fence



SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02	BY: Ruth Carver ROADWAY DEVELOPMENT ENGINEER



06-C HYDRANT SETTING DETAIL NO SCALE
CGDTHYD 24



06-D MANUFACTURED SLOPE INTERRUPTION AND PERIMETER CONTROL DETAIL NO SCALE
EC-DITCH-07 16

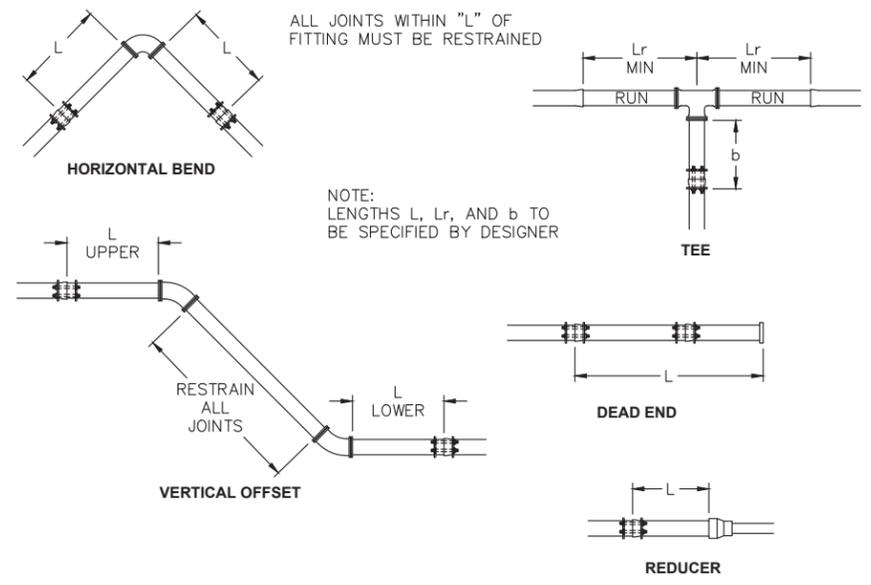
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 BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION
 CONSTRUCTION DETAILS
 CITY OF PEWAUKEE
 WAUKESHA COUNTY, WISCONSIN
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 SHEET NO. DT-01
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JOINT RESTRAINT DISTANCE (FT) PER FITTING FOR PVC AND DUCTILE IRON PIPE
(August 10, 2023)

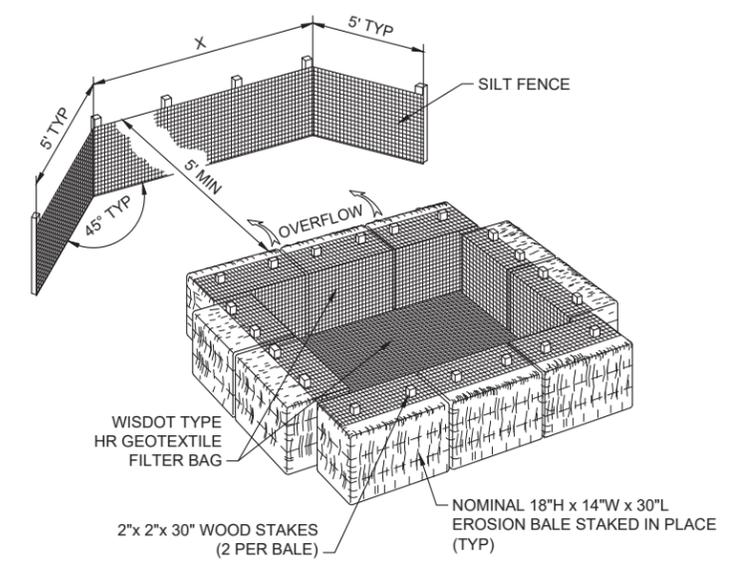
FITTING TYPE	4"	6"	8"	12"
Horizontal Bend - 11.25°	5	5	5	5
Horizontal Bend - 22.5°	8	8	8	10
Horizontal Bend - 45°	10	10	12	16
Horizontal Bend - 90°	16	20	25	32
Reducer - Diameter x 4"	-	32	59	102
Reducer - Diameter x 6"	-	-	35	85
Reducer - Diameter x 8"	-	-	-	62
Dead End	44	62	82	117
Length Along Run (Lr)	10	10	10	10
Tee - 4" Branch	8	6	6	6
Tee - 6" Branch	-	8	6	6
Tee - 8" Branch	-	-	10	6
Tee - 12" Branch	-	-	-	12
Vertical Bend - 45° - Upper	19	26	34	49
Vertical Bend - 45° - Lower	5	7	10	13
Vertical Bend - 22.5° - Upper	9	13	17	24
Vertical Bend - 22.5° - Lower	4	5	5	8
Vertical Bend - 11.25° - Upper	6	7	9	12
Vertical Bend - 11.25° - Lower	4	4	4	4

- 1.) Un-restrained sleeve fittings not allowed within the above distances.
- 2.) The specified joint restraint distance is required on both sides of the fitting unless otherwise indicated.
- 3.) The joint restraint distance shown is the minimum distance allowed. Joint restraints shall be extended to the first joint beyond the minimum distance shown.
- 4.) If a cross is installed with a reducer on one or more legs, joint restraints shall be installed per the tee fitting joint restraint length shown.



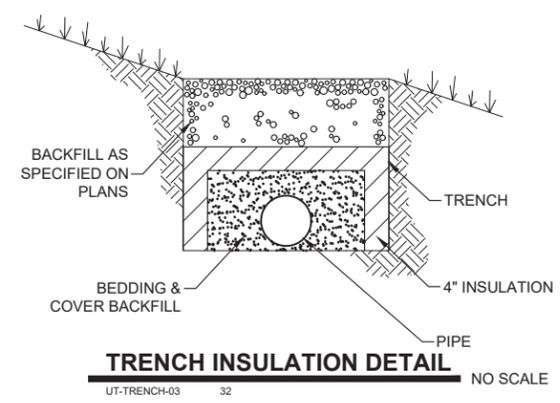
JOINT RESTRAINT DETAIL
WT-BR-03 4 NO SCALE

- NOTES:**
1. GEOTEXTILE BAG AND HAY BALE PERIMETER SHALL CONFORM TO WDNR TECHNICAL STANDARD 1061.
 2. ALL PUMPED EFFLUENT FROM DEWATERING OPERATIONS SHALL BE DISCHARGED TO GEOTEXTILE FILTER BAG.
 3. GEOTEXTILE BAG SHALL BE PLACED ON VEGETATED/STABILIZED GROUND AND DISCHARGE TO A SEDIMENT TRAP.
 4. DISCHARGE SHALL NOT BE ALLOWED TO FLOW OVER UNPROTECTED GROUND.
 5. IN NO CASE SHALL PUMPED WATER BE DIVERTED OUTSIDE PROJECT LIMITS PRIOR TO SEDIMENT REMOVAL.

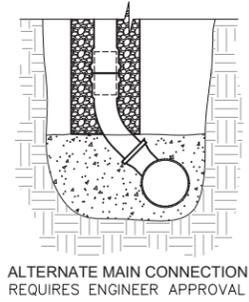


GEOTEXTILE SURFACE AREA = PUMP DISCHARGE RATE DIVIDED BY 1/2" PRODUCT WATER FLOW RATE.
MINIMUM GEOTEXTILE SURFACE AREA = 100 SQUARE FEET

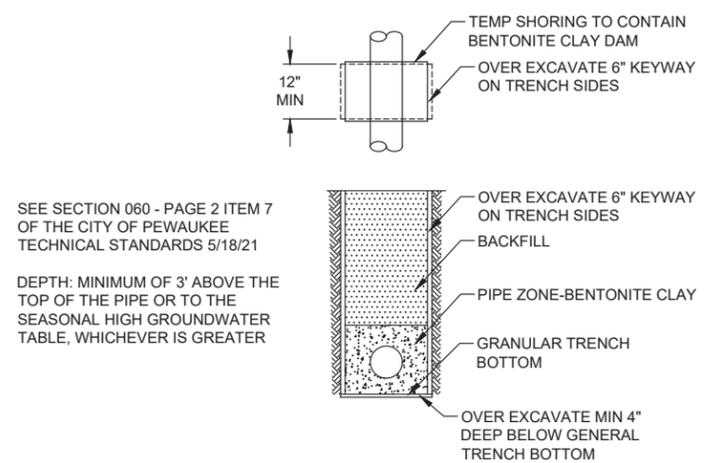
DEWATERING GEOTEXTILE FILTER BAG
EC-DW-01 1 NO SCALE



TRENCH INSULATION DETAIL
UT-TRENCH-03 32 NO SCALE



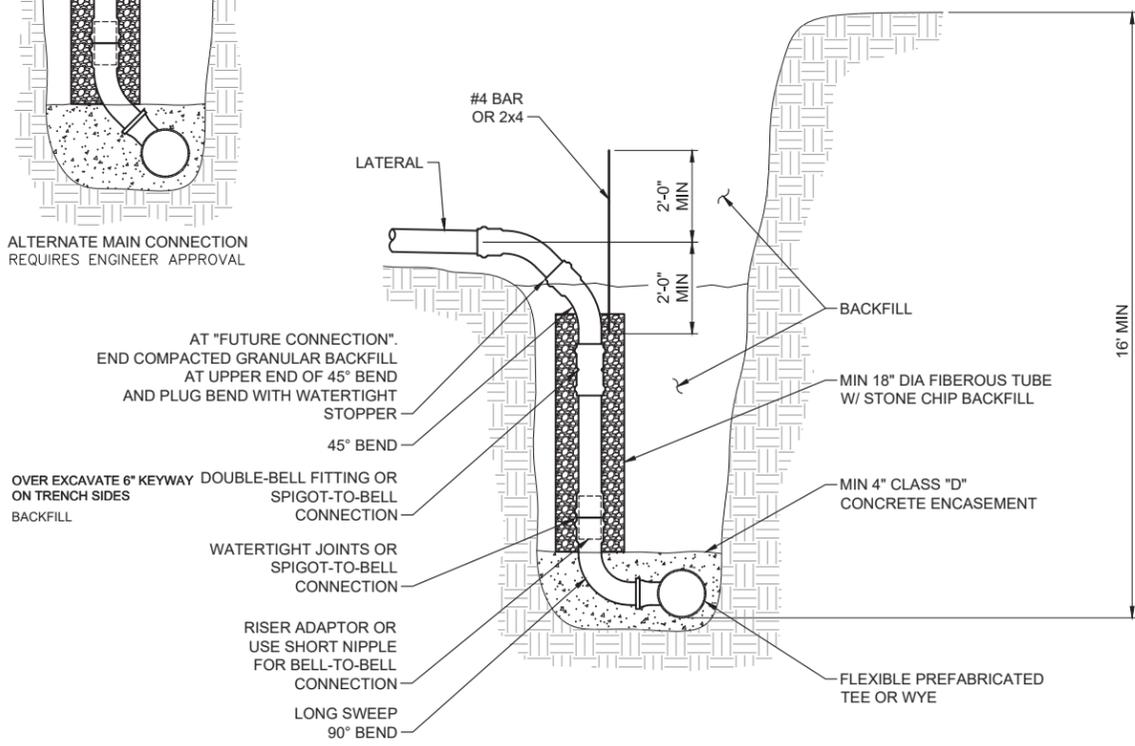
ALTERNATE MAIN CONNECTION REQUIRES ENGINEER APPROVAL



SEE SECTION 060 - PAGE 2 ITEM 7 OF THE CITY OF PEWAUKEE TECHNICAL STANDARDS 5/18/21

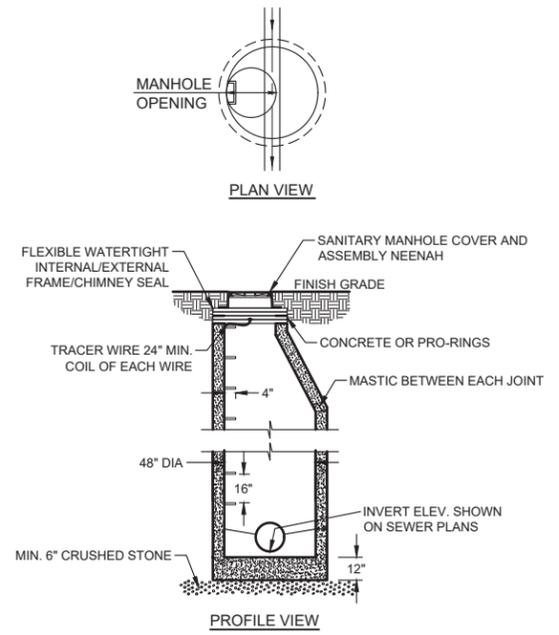
DEPTH: MINIMUM OF 3' ABOVE THE TOP OF THE PIPE OR TO THE SEASONAL HIGH GROUNDWATER TABLE, WHICHEVER IS GREATER

BENTONITE CLAY DAM
UT-TRENCH-05 24 NO SCALE



FOLLOW THIS FOR RISERS GREATER THAN 6 FT HIGH OR MAINS GREATER THAN 16 FT DEEP

FLEXIBLE RISER TO FLEXIBLE SEWER MAIN
SAN-RISER-01 24 NO SCALE



- NOTES:**
1. TRACER WIRE TO BE BROUGHT INTO MANHOLE BETWEEN ADJUSTING RINGS. MINIMUM OF 24" TRACER WIRE TO BE COILED HUNG ON STEP IN MANHOLE
 2. MANHOLE OPENING TO BE LOCATED OVER STEPS.
 3. MANHOLE STEPS SHALL NOT BE LOCATED OVER SEWER.

TYPICAL SANITARY SEWER MANHOLE DETAIL
Sanitary MH typ opening offset 1 NO SCALE

Mar 11, 2025 3:07pm PLOTTED BY DKlemm - SAVED BY DKlemm
 IMAGES: RM SQUARE - Full Cover-Print: OFFICES: EC-DW-01: PV-SECT-07: WT-BR-03: Sanitary MH typ opening offset: Class: Erosion/Blanket: SAN-RISER-01: UT-TRENCH-05: Cops: MM Bridge Attachment: 01: UT-TRENCH-07: PV-CURB-15: G:\CS2_201828_City of Pewaukee\1114 Blumens Road Water Main Extension\1114 Blumens Road Water Main Extension\1114 Blumens Road Water Main Extension.dwg

Ruekert Mielke
Waukesha Kenosha Madison
Global Water Center Fox Valley
www.ruekertmielke.com

BLUEMOUND ROAD WATER & SEWER - TAKOMA EXTENSION

CONSTRUCTION DETAILS

CITY OF PEWAUKEE
WAUKESHA COUNTY, WISCONSIN

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DESIGNED BY: GRC

DRAFTED BY: AK2

CHECKED BY: VVR

DATE: MARCH 2025

FILE NO.
26-10144.200

BID SET

SHEET NO.
DT-02

www.ruekertmielke.com

