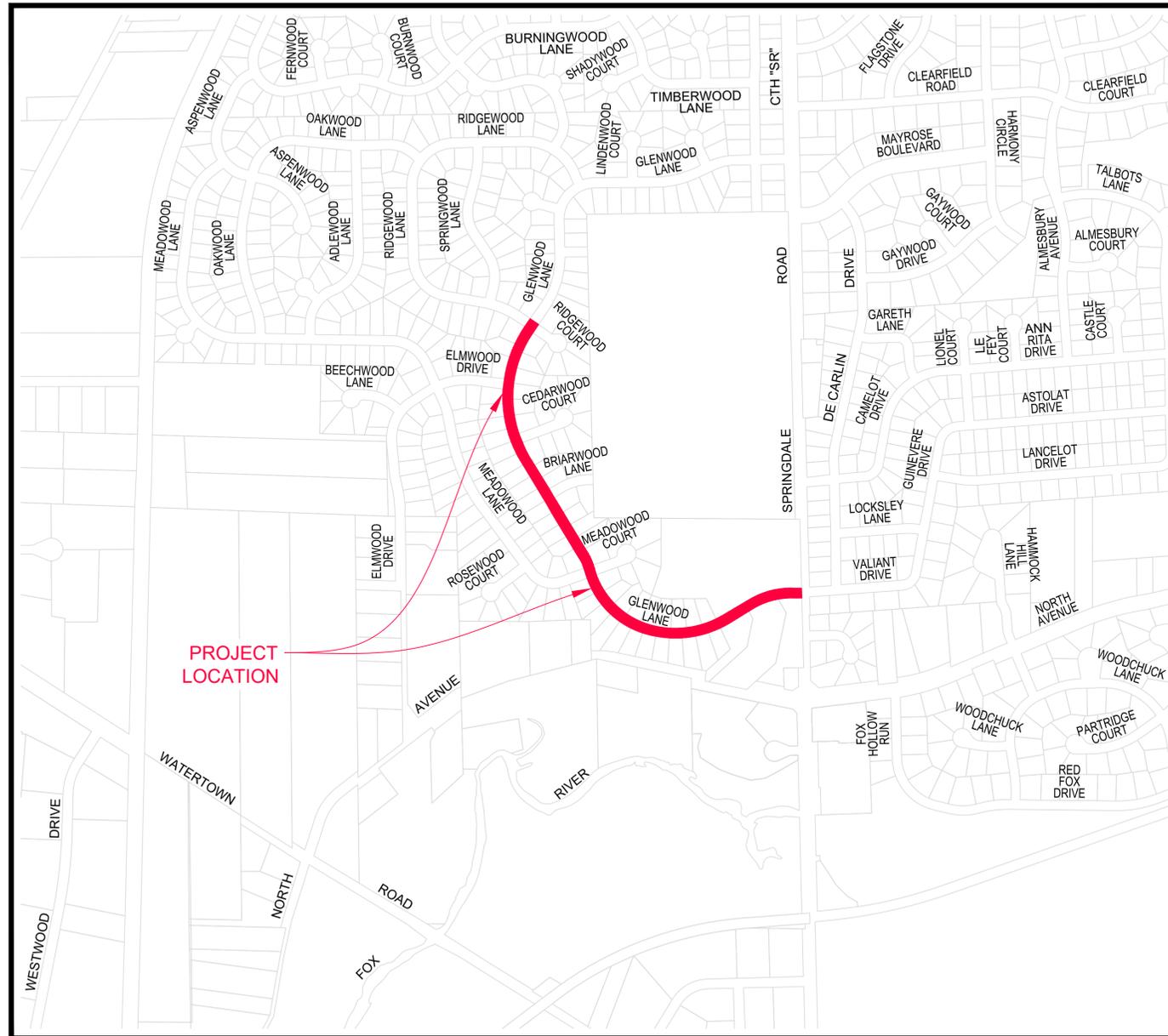




# GLENWOOD LANE RECONSTRUCTION

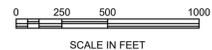
CITY PROJECT NUMBER: 2026.002.E  
CITY OF PEWAUKEE  
WAUKESHA COUNTY, WISCONSIN



PROJECT LOCATION

LOCATION MAP

TOWN	RANGE	SECTION (s)
7N	19E	13 SE & 24 NE



SCALE IN FEET  
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## LEGEND - CIVIL ENGINEERING DRAWINGS

CONTROL	SANITARY SEWER	UTILITIES	WATER MAIN
<ul style="list-style-type: none"> <li>BENCHMARK</li> <li>CHISELED MARK</li> <li>CONTROL POINT</li> <li>IRON PIPE (GENERIC)</li> <li>IRON PIPE (1" DIA)</li> <li>IRON PIPE (2" DIA)</li> <li>IRON PIPE SET</li> <li>IRON ROD (GENERIC)</li> <li>IRON ROD (3/4" DIA)</li> <li>IRON ROD (1 1/4" DIA)</li> <li>MONUMENT</li> <li>NAIL</li> <li>PAINT MARK</li> <li>PK NAIL</li> <li>RAILROAD SPIKE</li> <li>REFERENCE TIE</li> <li>SECTION CORNER</li> <li>SECTION CORNER MONUMENT</li> <li>TEMPORARY CONTROL POINT</li> <li>USGS MONUMENT</li> </ul>	<ul style="list-style-type: none"> <li>SANITARY MANHOLE</li> <li>SEPTIC</li> <li>SEPTIC TANK COVER</li> <li>SEPTIC VENT</li> <li>SANITARY SEWER</li> <li>PROPOSED SANITARY CLEANOUT</li> <li>PROPOSED SANITARY MANHOLE</li> <li>PROPOSED SANITARY RISER</li> <li>PROPOSED SANITARY LATERAL</li> <li>PROPOSED SANITARY SEWER</li> <li>STORM SEWER</li> <li>STORM CATCH BASIN</li> <li>STORM FIELD INLET</li> <li>STORM INLET</li> <li>STORM MANHOLE</li> <li>STORM YARD DRAIN</li> <li>STORM SEWER</li> <li>CULVERT (SIZE &amp; TYPE NOTED)</li> <li>PROPOSED STORM CLEANOUT</li> <li>PROPOSED STORM FIELD INLET</li> <li>PROPOSED STORM INLET</li> <li>PROPOSED STORM MANHOLE</li> <li>PROPOSED STORM ROOF DRAIN</li> <li>PROPOSED STORM YARD DRAIN</li> <li>PROPOSED STORM LATERAL</li> <li>PROPOSED STORM SEWER</li> </ul>	<ul style="list-style-type: none"> <li>CABLE BOX</li> <li>CABLE MANHOLE</li> <li>UNDERGROUND TV CABLE</li> <li>ELECTRIC BOX</li> <li>ELECTRIC MANHOLE</li> <li>ELECTRIC METER</li> <li>ELECTRIC PAD</li> <li>ELECTRIC TRANSFORMER</li> <li>POWER POLE</li> <li>UNDERGROUND ELECTRIC</li> <li>GAS CURB STOP</li> <li>GAS MANHOLE</li> <li>GAS METER</li> <li>GAS TEST STATION</li> <li>GAS VALVE</li> <li>GAS VALVE TEST</li> <li>GAS VENT</li> <li>UNDERGROUND GAS MAIN</li> <li>TELEPHONE BOX</li> <li>TELEPHONE MANHOLE</li> <li>UNDERGROUND TELEPHONE</li> <li>UNDERGROUND FIBER OPTIC</li> </ul>	<ul style="list-style-type: none"> <li>EXISTING HYDRANT</li> <li>EXISTING LOCATOR BOX</li> <li>EXISTING SPRINKLER HEAD</li> <li>EXISTING WATER CURB STOP</li> <li>EXISTING WATER MAIN BEND</li> <li>EXISTING WATER MAIN CROSS</li> <li>EXISTING WATER MAIN OFFSET</li> <li>EXISTING WATER MAIN PLUG</li> <li>EXISTING WATER MAIN PLUG W/ AIR RELEASE</li> <li>EXISTING WATER MAIN REDUCER</li> <li>EXISTING WATER VALVE</li> <li>EXISTING WATER VALVE MANHOLE</li> <li>EXISTING WELL</li> <li>EXISTING YARD HYDRANT</li> <li>EXISTING WATER MAIN</li> <li>EXISTING WATER SERVICE</li> <li>PROPOSED CURB STOP</li> <li>PROPOSED HYDRANT</li> <li>PROPOSED LOCATOR BOX</li> <li>PROPOSED WATER MAIN BEND (ANGLE NOTED)</li> <li>PROPOSED WATER MAIN CROSS</li> <li>PROPOSED WATER MAIN OFFSET</li> <li>PROPOSED WATER MAIN PLUG</li> <li>PROPOSED WATER MAIN PLUG W/AIR RELEASE</li> <li>PROPOSED WATER MAIN REDUCER</li> <li>PROPOSED WATER MAIN TEE</li> <li>PROPOSED WATER VALVE</li> <li>PROPOSED WATER VALVE MANHOLE</li> <li>PROPOSED YARD HYDRANT</li> <li>PROPOSED WATER MAIN</li> <li>PROPOSED WATER SERVICE</li> </ul>
<ul style="list-style-type: none"> <li>GROUND</li> <li>WATER ELEVATION</li> <li>WETLANDS (SURVEYED LOCATION)</li> <li>CROPPED FIELD</li> <li>DITCH</li> <li>GUARD RAIL</li> <li>TOE OF SLOPE</li> <li>TOP OF BANK</li> <li>WETLAND BOUNDARY</li> <li>EROSION CONTROL</li> <li>PROPOSED DITCH CHECK</li> <li>PROPOSED EROSION LOGS/EROSION WADDLES</li> <li>PROPOSED INLET PROTECTION TYPE A</li> <li>PROPOSED INLET PROTECTION TYPE B</li> <li>PROPOSED INLET PROTECTION TYPE C</li> <li>PROPOSED INLET PROTECTION TYPE D</li> <li>PROPOSED BARRIER FENCE</li> <li>PROPOSED SILT FENCE</li> <li>PROPOSED EROSION MAT</li> <li>PROPOSED RIP RAP</li> <li>PROPOSED TRACKING PAD</li> </ul>	<ul style="list-style-type: none"> <li>GENERAL</li> <li>AIR CONDITIONER</li> <li>CLEAN OUT</li> <li>DELINEATOR POST</li> <li>FLAG POLE</li> <li>GENERIC HAND HOLE</li> <li>GENERIC MANHOLE</li> <li>GENERIC METER</li> <li>GENERIC PEDESTAL</li> <li>GENERIC VALVE</li> <li>GENERIC VENT</li> <li>GUY POLE</li> <li>GUY WIRE</li> <li>LIGHT POLE</li> <li>MAIL BOX</li> <li>MARKER POST</li> <li>MONITORING WELL</li> <li>PILE</li> <li>POST</li> <li>REVISION LABEL</li> <li>SOIL BORING</li> <li>UTILITY POLE</li> <li>YARD LIGHT</li> </ul>	<ul style="list-style-type: none"> <li>LANDSCAPE</li> <li>CONIFEROUS MULTIPLE TRUNK TREE</li> <li>CONIFEROUS TREE</li> <li>DECIDUOUS TREE</li> <li>DECIDUOUS MULTIPLE TRUNK TREE</li> <li>DECORATIVE ROCK</li> <li>STUMP</li> <li>EDGE OF PLANTER/LANDSCAPE BED</li> <li>EDGE OF TREES &amp; BRUSH</li> <li>FENCE</li> <li>RAILROAD</li> <li>RAILROAD SIGNAL BOX</li> <li>RAILROAD SIGNAL FLASHER</li> <li>RAILROAD SPIKE</li> <li>RAILROAD TRACKS</li> <li>TRAFFIC SIGNAL</li> <li>CONTROL BOX/LIGHTING CABINET</li> <li>PULL BOX</li> <li>TRAFFIC SIGNAL</li> <li>UNDERGROUND LOOP DETECTOR</li> </ul>	<ul style="list-style-type: none"> <li>PAVEMENT</li> <li>EXISTING SIGN</li> <li>EDGE OF ASPHALT PAVEMENT</li> <li>EDGE OF GRAVEL PAVEMENT</li> <li>EDGE OF CONCRETE PAVEMENT</li> <li>PROPOSED DETECTABLE WARNING FIELD</li> <li>PROPOSED SIGN</li> <li>PROPOSED EDGE OF ASPHALT PAVEMENT</li> <li>PROPOSED EDGE OF CONCRETE PAVEMENT</li> <li>PROPOSED EDGE OF GRAVEL PAVEMENT</li> <li>PROPOSED SLOPE INTERCEPT</li> </ul>

ORIGINAL PLAN SET IS IN COLOR



CHECKED BY: *BEF*

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PROJECT NUMBER: 26-10174.200



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# GLENWOOD LANE RECONSTRUCTION

CITY PROJECT NUMBER: 2026.002.E  
CITY OF PEWAUKEE  
WAUKESHA COUNTY, WISCONSIN

## SHEET INDEX

SHEET NO.	LOCATION	DESCRIPTION
<b>GENERAL</b>		
INDEX-01		INDEX SHEET
GN-01		GENERAL NOTES
TS-01		TYPICAL SECTIONS
<b>EROSION CONTROL</b>		
EC-01 TO EC-04		PROPOSED EROSION CONTROL - GLENWOOD LANE
<b>PAVEMENT &amp; STORM SEWER</b>		
PS-01	IN: GLENWOOD LANE PROPOSED PAVEMENT & STORM SEWER	TO: 500' WEST OF SPRINGDALE ROAD/STH "SR" FROM: SPRINGDALE ROAD/STH "SR"
PS-02	IN: GLENWOOD LANE PROPOSED PAVEMENT & STORM SEWER	FROM: 500' WEST OF SPRINGDALE ROAD/CTH "SR" TO: 1000' WEST OF SPRINGDALE ROAD/CTH "SR"
PS-03	IN: GLENWOOD LANE PROPOSED PAVEMENT & STORM SEWER	FROM: 1000' WEST OF SPRINGDALE ROAD/CTH "SR" TO: 150' SOUTHEAST OF MEADOWOOD LANE/MEADOWOOD COURT
PS-04	IN: GLENWOOD LANE PROPOSED PAVEMENT & STORM SEWER	FROM: 150' SOUTHEAST OF MEADOWOOD LANE/MEADOWOOD COURT TO: 150' SOUTHEAST OF BRIARWOOD LANE
PS-05	IN: GLENWOOD LANE PROPOSED PAVEMENT & STORM SEWER	FROM: 150' SOUTHEAST OF BRIARWOOD LANE TO: 50' SOUTHEAST OF CEDARWOOD COURT
PS-06	IN: GLENWOOD LANE PROPOSED PAVEMENT & STORM SEWER	FROM: 50' SOUTHEAST OF CEDARWOOD COURT TO: ELMWOOD DRIVE
PS-07	IN: GLENWOOD LANE PROPOSED PAVEMENT & STORM SEWER	FROM: ELMWOOD DRIVE TO: RIDGEWOOD LANE/RIDGEWOOD COURT
<b>INTERSECTION DETAILS</b>		
ID-01 & ID-02		INTERSECTION DETAIL - GLENWOOD LANE
<b>CONSTRUCTION DETAILS</b>		
DT-01 TO DT-04		CONSTRUCTION DETAILS
<b>CROSS SECTIONS</b>		
CS-01 TO CS-26		GLENWOOD LANE CROSS SECTIONS

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GLENWOOD LANE RECONSTRUCTION  
INDEX SHEET  
CITY OF PEWAUKEE  
WAUKESHA COUNTY, WISCONSIN

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DESIGNED BY: VVR

DRAFTED BY: AK2

CHECKED BY: BEF

DATE: FEBRUARY 2026

FILE NO.  
**26-10174.200**

**BID SET**  
SHEET NO.  
**INDEX-01**

**EROSION CONTROL NOTES**

- POST WDNR CERTIFICATE OF PERMIT COVERAGE ON SITE AND MAINTAIN UNTIL CONSTRUCTION ACTIVITIES HAVE CEASED, THE SITE IS STABILIZED, AND A NOTICE OF TERMINATION IS FILED WITH WDNR.
- KEEP A COPY OF THE CURRENT EROSION CONTROL PLAN ON SITE THROUGHOUT THE DURATION OF THE PROJECT.
- ROUTINE SITE INSPECTIONS OF ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE DONE AT LEAST ONCE EVERY 7 DAYS AND WITHIN 24 HOURS AFTER A RAINFALL EVENT OF 0.5 INCHES OR GREATER. CONTRACTOR SHALL REPAIR DEFICIENT EROSION AND SEDIMENT CONTROL MEASURES WITHIN 24-HOURS AFTER INSPECTION. KEEP INSPECTION REPORTS ON-SITE AND MAKE THEM AVAILABLE UPON REQUEST.
- INSPECT AND MAINTAIN ALL INSTALLED EROSION CONTROL PRACTICES UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED.
- WHEN POSSIBLE: PRESERVE EXISTING VEGETATION (ESPECIALLY ADJACENT TO SURFACE WATERS), MINIMIZE LAND-DISTURBING CONSTRUCTION ACTIVITY ON SLOPES OF 20% OR MORE, MINIMIZE SOIL COMPACTION, AND PRESERVE TOPSOIL.
- FOLLOW 01 57 13 AND REFER TO THE WDNR STORMWATER CONSTRUCTION TECHNICAL STANDARDS AT [http://dnr.wi.gov/topic/stormwater/standards/const\\_standards.html](http://dnr.wi.gov/topic/stormwater/standards/const_standards.html).
- INSTALL PERIMETER EROSION CONTROLS AND ROCK TRACKING PAD CONSTRUCTION ENTRANCE(S) PRIOR TO ANY LAND-DISTURBING ACTIVITIES, INCLUDING CLEARING AND GRUBBING. USE **WDNR TECHNICAL STANDARD STONE TRACKING PAD AND TIRE WASHING #1057** FOR ROCK CONSTRUCTION ENTRANCE(S).
- INSTALL INLET PROTECTION PRIOR TO LAND-DISTURBING ACTIVITIES IN THE CONTRIBUTING DRAINAGE AREA AND/OR IMMEDIATELY UPON INLET INSTALLATION. COMPLY WITH **WDNR TECHNICAL STANDARD STORM INLET PROTECTION FOR CONSTRUCTION SITES #1060**.
- STAGE CONSTRUCTION GRADING ACTIVITIES TO MINIMIZE THE CUMULATIVE EXPOSED AREA. CONDUCT TEMPORARY GRADING FOR EROSION CONTROL PER **WDNR TECHNICAL STANDARD TEMPORARY GRADING PRACTICES FOR EROSION CONTROL #1067**. MAINTAIN SITE DRAINAGE AT ALL TIMES DURING CONSTRUCTION.
- PERMITTING OF GROUNDWATER DEWATERING IS THE RESPONSIBILITY OF CONTRACTOR. GROUNDWATER DEWATERING IS SUBJECT TO A DNR WASTEWATER DISCHARGE PERMIT AND A DNR HIGH CAPACITY WELL APPROVAL IF CUMULATIVE PUMP CAPACITY IS 70 GPM OR MORE.
- PROVIDE ANTI-SCOUR PROTECTION AND MAINTAIN NON-EROSIVE FLOW DURING DEWATERING. PERFORM DEWATERING OF ACCUMULATED SURFACE RUNOFF IN ACCORDANCE WITH **WDNR TECHNICAL STANDARD DE-WATERING #1061**.
- INSTALL AND MAINTAIN SILT FENCING PER **WDNR TECHNICAL STANDARD SILT FENCE #1056**. REMOVE SEDIMENT FROM BEHIND SILT FENCES AND SEDIMENT BARRIERS BEFORE SEDIMENT REACHES A DEPTH THAT IS EQUAL TO ONE-HALF OF THE FENCE AND/OR BARRIER HEIGHT.
- REPAIR BREAK AND GAPS IN SILT FENCES AND BARRIERS IMMEDIATELY. REPLACE DECOMPOSING STRAW BALES (TYPICAL BALE LIFE IS 3 MONTHS). LOCATE, INSTALL, AND MAINTAIN STRAW BALES PER **WDNR TECHNICAL STANDARD DITCH CHECKS #1062**.
- INSTALL AND MAINTAIN FILTER SOCKS IN ACCORDANCE WITH **WDNR TECHNICAL STANDARD INTERIM MANUFACTURED PERIMETER CONTROL AND SLOPE INTERRUPTION PRODUCTS #1071**.
- IMMEDIATELY STABILIZE STOCKPILES AND SURROUND STOCKPILES AS NEEDED WITH SILT FENCE OR OTHER PERIMETER CONTROL IF STOCKPILES WILL REMAIN INACTIVE FOR 7 DAYS OR LONGER.
- IMMEDIATELY STABILIZE ALL DISTURBED AREAS THAT WILL REMAIN INACTIVE FOR 14 DAYS OR LONGER. FOLLOW 01 57 13 FOR LATE SEASON STABILIZATION AFTER SEPTEMBER 15TH.
- STABILIZE AREAS OF FINAL GRADING WITHIN 7 DAYS OF REACHING FINAL GRADE.
- SWEEP/CLEAN UP ALL SEDIMENT/TRASH THAT MOVES OFF-SITE DUE TO CONSTRUCTION ACTIVITY OR STORM EVENTS BEFORE THE END OF THE SAME WORKDAY OR AS DIRECTED BY OWNER & ENGINEER. SEPARATE SWEPT MATERIALS (SOILS AND TRASH) AND DISPOSE OF APPROPRIATELY.
- CONTRACTOR IS RESPONSIBLE FOR CONTROLLING DUST PER **WDNR TECHNICAL STANDARD DUST CONTROL ON CONSTRUCTION SITES #1068**.
- PROPERLY DISPOSE OF ALL WASTE AND UNUSED BUILDING MATERIALS (INCLUDING GARBAGE, DEBRIS, CLEANING WASTES, OR OTHER CONSTRUCTION MATERIALS) AND DO NOT ALLOW THESE MATERIALS TO BE CARRIED BY RUNOFF INTO THE RECEIVING CHANNEL.
- COORDINATE WITH MUNICIPALITY TO UPDATE THE LAND DISTURBANCE PERMIT TO INDICATE THE ANTICIPATED OR LIKELY DISPOSAL LOCATIONS FOR ANY EXCAVATED SOILS OR CONSTRUCTION DEBRIS THAT WILL BE HAULED OFF-SITE FOR DISPOSAL. THE DEPOSITED OR STOCKPILED MATERIAL NEEDS TO INCLUDE PERIMETER SEDIMENT CONTROL MEASURES (SUCH AS SILT FENCE, HAY BALES, FILTER SOCKS, OR COMPACTED EARTHEN BERMS).
- FOR NON-CHANNELIZED FLOW ON DISTURBED OR CONSTRUCTED SLOPES, PROVIDE CLASS 1, URBAN TYPE A EROSION CONTROL MATTING. INSTALL AND MAINTAIN PER **WDNR TECHNICAL STANDARD NON-CHANNEL EROSION MAT #1052**.
- FOR CHANNELIZED FLOW ON DISTURBED OR CONSTRUCTED AREAS, PROVIDE CLASS 1, URBAN TYPE B EROSION CONTROL MATTING. INSTALL AND MAINTAIN PER **WDNR TECHNICAL STANDARD CHANNEL EROSION MAT #1053**.
- MAKE PROVISIONS FOR WATERING. FOLLOW 32 90 00 FOR PERMANENT RESTORATION. FOLLOW **WDNR TECHNICAL STANDARD SEED FOR CONSTRUCTION SITE EROSION CONTROL #1059** FOR TEMPORARY SEEDING.
- INSTALL ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES IF NECESSARY OR AS DIRECTED BY OWNER OR ENGINEER.
- CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE WDNR REMEDIATION AND WASTE MANAGEMENT REQUIREMENTS FOR HANDLING AND DISPOSING OF CONTAMINATED MATERIALS.

**TRAFFIC CONTROL NOTES**

- ACCESS TO ALL DRIVEWAYS AND ADJACENT STREETS SHALL BE MAINTAINED.
- TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED THROUGH COMPLETION OF EACH STATE OF THE WORK.
- FOLLOW ALL WISDOT AND MUTCD GUIDELINES.
- TYPE III BARRICADES SHALL HAVE TWO TYPE A LIGHTS ON EACH BARRICADE AND HAVE A WIDTH OF 8 FEET.
- ALL SIGNS WHICH CONFLICT WITH THE TRAFFIC CONTROL SIGNING SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.
- GLENWOOD LANE SOUTH OF MEADOWOOD LANE SHALL BE CLOSED TO TRAFFIC AT THE INTERSECTION OF GLENWOOD LANE AND MEADOWOOD LANE DURING CONSTRUCTION OF THE BOX CULVERT.
- CONTRACTOR SHALL SUBMIT TO ENGINEER A DETAILED TRAFFIC CONTROL PLAN AND DETOUR PLAN FOR CLOSURE OF GLENWOOD LANE, SOUTH OF MEADOWOOD LANE, FOR CONSTRUCTION OF THE BOX CULVERT. TRAFFIC CONTROL PLAN FOR ACCESS TO LOCAL TRAFFIC THROUGH THE WORK ZONE AND DETOUR PLAN SHALL BE REVIEWED AND APPROVED BY ENGINEER PRIOR TO START OF THE WORK.
- PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) SHALL BE PLACED A MINIMUM OF 7 DAYS IN ADVANCE OF UPCOMING TRAFFIC PATTERN CHANGES (PRIOR TO CONSTRUCTION OF BOX CULVERT).

**GENERAL NOTES**

- LOCATION OF UNDERGROUND FACILITIES SHOWN ARE BASED UPON AVAILABLE RECORDS AND ARE NOT GUARANTEED TO BE COMPLETE OR CORRECT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ALL PUBLIC AND PRIVATE UTILITIES TO DETERMINE THE EXACT LOCATION OF ALL FACILITIES BEFORE BEGINNING WORK.
- CONTRACTOR SHALL EXPOSE AND VERIFY THE LOCATION AND DEPTH OF EXISTING UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION OF NEW UNDERGROUND UTILITIES.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF ALL EXISTING UTILITIES DURING CONSTRUCTION. ANY UTILITY DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED IN ACCORDANCE WITH THE RELATED UTILITIES SPECIFICATION AT THE CONTRACTORS EXPENSE.

**CONSTRUCTION SEQUENCE**

- INSTALL WORK ZONE TRAFFIC CONTROL MEASURES AS APPROVED BY CITY.
- INSTALL AND MAINTAIN EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO ANY LAND DISTURBING ACTIVITIES, AS SHOW ON DRAWINGS AND DIRECTED BY ENGINEER. MAINTAIN MEASURES AT THE END OF EACH DAY.
- CLEAR AND GRUB VEGETATION AS SHOWN ON DRAWINGS OR AS DIRECTED BY ENGINEER
- TREE REMOVALS WILL BE MARKED PRIOR TO START OF WORK. DO NOT REMOVE TREES NOT MARKED FOR REMOVAL.
- BEGIN CONSTRUCTION ACTIVITIES AS SHOWN ON DRAWINGS AND SPECIFICATIONS.
- BOX CULVERT CONSTRUCTION MAY NOT PROCEED PRIOR TO RECEIPT OF WDNR WETLAND DISTURBANCE PERMIT. ONCE PERMIT IS RECEIVED, INSTALL DIVERSION MEASURES JUST PRIOR TO BOX CULVERT INSTALLATION.
- COMPLETE BOX CULVERT INSTALLATION AND REMOVE STREAM DIVERSION MEASURES IMMEDIATELY UPON COMPLETION OF BOX CULVERT INSTALLATION.
- COMPLETE UTILITY AND ROAD INSTALLATION, BEFORE OR AFTER BOX CULVERT INSTALLATION.
- STABILIZE NEWLY GRADED AND DISTURBED AREAS WITHIN 7 DAYS OF BEING INACTIVE.
- COMPLETE FINAL RESTORATION INCLUDING BUT NOT LIMITED TO TOPSOIL, SEEDING, MULCH, AND EROSION MAT.
- REMOVE TEMPORARY EROSION CONTROL DEVICES AFTER 70% GROWTH DENSITY HAS OCCURRED IN 100% OF RESTORATION AREAS. CONTRACTOR SHALL OBTAIN OWNER AND ENGINEER APPROVAL PRIOR TO REMOVING THE MEASURES. RESTORE DISTURBED AREAS AROUND DEVICES AND CLEAN OUT STORM WATER STRUCTURES. PERFORM FINAL SITE CLEANING.

**BOX CULVERT CONSTRUCTION NOTES**

- CULVERT CONSTRUCTION SHALL TAKE PLACE AFTER JUNE 15 AND DURING DRY WEATHER ONLY.
- CONTRACTOR SHALL SUBMIT TO ENGINEER A WRITTEN DETAILED DIVERSION MANAGEMENT PLAN FOR CONVEYING FLOWS DURING CONSTRUCTION AND EROSION CONTROL. DIVERSION MANAGEMENT PLAN SHALL BE REVIEWED AND APPROVED BY ENGINEER PRIOR TO START OF THE WORK
- CONTRACTOR SHALL FOLLOW APPROVED DIVERSION MANAGEMENT PLAN FOR APPROVED CONSTRUCTION ACTIVITIES AND REQUIRED EROSION CONTROL DEVICES.
- INSTALL AND TEST TEMPORARY BYPASS LIMIT BARRIERS AND BYPASS PUMPING SYSTEM. BYPASS PUMPING SYSTEM SHALL BE SIZED TO MAINTAIN WATER ELEVATION WITHIN THE DRAINAGE WAY DURING CONSTRUCTION. THE ANTICIPATED FLOW DURING A 100-YEAR EVENT IS 240 CFS.
- REMOVE WATER FROM EXISTING STORM SEWER.
- REMOVE EXISTING STORM SEWER AND INSTALL PROPOSED WATER MAIN OFFSET AND BOX CULVERT.
- RESTORE WORK AREA. REMOVE TEMPORARY LIMIT BARRIERS AND TEMPORARY PUMPING SYSTEM AND APPURTENANCES FROM THE SITE.

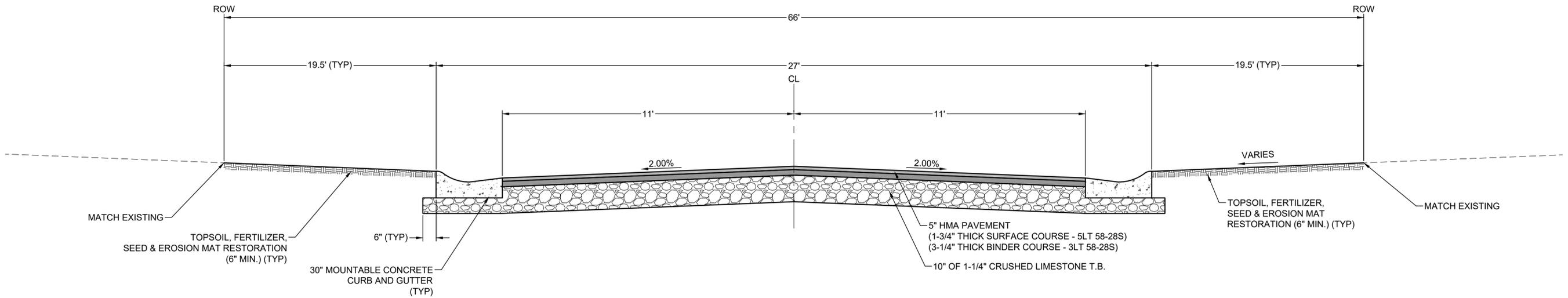
**Table 1 - Prescriptive Compliance Area Soil Stabilization**

Prescriptive Compliance Areas	Bare Soil	Slope & Channel Management	Periods of Inactivity	Final Grade
Soil stockpiles that will exist for more than 7 days	<p><u>Areas that Do Not Drain to Sediment Basins or Traps</u></p> <p>Limit the duration of soil exposure to no more than 30 days,</p>	<p><u>General</u></p> <p>Design and implement approved soil stabilization practices per DNR technical standards.</p> <p>Refer to WISDOT Slope &amp; Channel Matrices for appropriate slope and slope length conditions.</p> <p><u>Slope Steeper than 20%</u></p> <p>Provide stable diversion of off-site runoff around the slope.</p> <p>Provide slope interruption devices in accordance with Manufactured Perimeter Control &amp; Slope Interruption Products technical Standard 1071 or equivalent methods to reduce uninterrupted slope length.</p>	<p><u>Planned Inactivity</u></p> <p>Stabilize immediately if area will be left inactive for more than 14 days.</p>	<p><u>Permanent Features</u></p> <p>Stabilize area immediately after reaching final grade.</p>
Utility trench backfills			<p><u>Unplanned Inactivity</u></p> <p>Stabilize area immediately if period of inactivity reaches 14 days.</p>	<p><u>Temporary Features</u></p> <p>Stabilize area immediately after establishment of temporary feature or reaching specified temporary grade.</p>
Temporary ditches/swales that will exist for more than 7 days	<p><u>Areas that Drain to Sediment Basins or Traps</u></p> <p>Limit the duration of soil exposures to no more than 90 days. However, use the duration from the soil loss and sediment discharge calculations for the other areas of the site if less than 90 days.</p>	<p><u>Slope Steeper than 20%</u></p> <p>Provide stable diversion of off-site runoff around the slope.</p> <p>Provide slope interruption devices in accordance with Manufactured Perimeter Control &amp; Slope Interruption Products technical Standard 1071 or equivalent methods to reduce uninterrupted slope length.</p>	<p><u>Unplanned Inactivity</u></p> <p>Stabilize area immediately if period of inactivity reaches 14 days.</p>	<p><u>Temporary Features</u></p> <p>Stabilize area immediately after establishment of temporary feature or reaching specified temporary grade.</p>
Permanent ditches/swales				
Small Areas - Less than 1 acre and less than 1% of site				
Discrete Areas - Less than 1 acre	<p><u>Areas that Drain to Sediment Basins or Traps</u></p> <p>Limit the duration of soil exposures to no more than 90 days. However, use the duration from the soil loss and sediment discharge calculations for the other areas of the site if less than 90 days.</p>	<p><u>Slope Steeper than 20%</u></p> <p>Provide stable diversion of off-site runoff around the slope.</p> <p>Provide slope interruption devices in accordance with Manufactured Perimeter Control &amp; Slope Interruption Products technical Standard 1071 or equivalent methods to reduce uninterrupted slope length.</p>	<p><u>Unplanned Inactivity</u></p> <p>Stabilize area immediately if period of inactivity reaches 14 days.</p>	<p><u>Temporary Features</u></p> <p>Stabilize area immediately after establishment of temporary feature or reaching specified temporary grade.</p>
Storm water practice side slopes				
Slopes Steeper than 20%				

GLENWOOD LANE RECONSTRUCTION  
 GENERAL NOTES  
 CITY OF PEWAUKEE  
 WAUKESHA COUNTY, WISCONSIN

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 CHECKED BY: BEF  
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 SHEET NO. GN-01

Feb 13, 2026 9:20am PLOTTED BY: AKrueger SAVED BY: AKrueger  
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 ZC: TYPICAL SECTIONS-01.dwg



**GLENWOOD LANE  
RESIDENTIAL STREET CROSS SECTION**

NO SCALE

Cgdt-Typical Section-01 2

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**GLENWOOD LANE RECONSTRUCTION**  
 TYPICAL SECTIONS  
 CITY OF PEWAUKEE  
 WAUKESHA COUNTY, WISCONSIN

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 FILE NO.  
**26-10174.200**  
**BID SET**  
 SHEET NO.  
**TS-01**

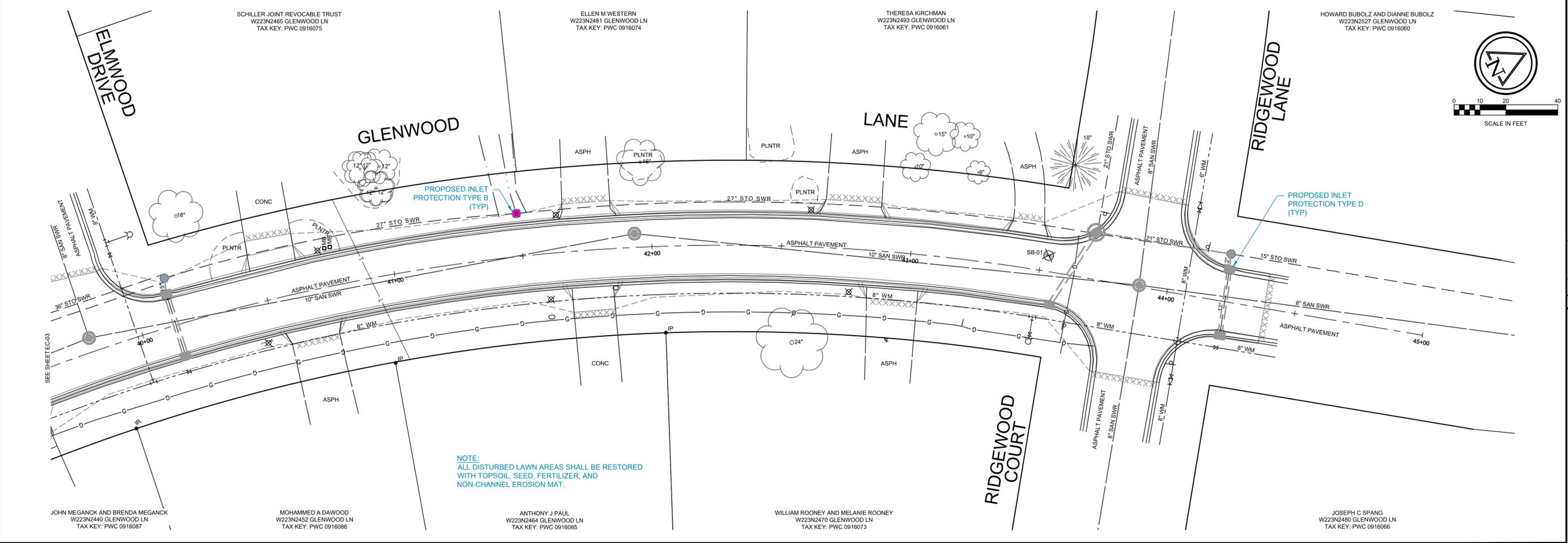
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**GLENWOOD LANE RECONSTRUCTION**  
**PROPOSED EROSION CONTROL - GLENWOOD LANE**  
**CITY OF PEWAUKEE**  
**WAUKESHA COUNTY, WISCONSIN**

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BID SET



24P-GLENWOOD LANE-PS-02 Layout

CITY OF PEWAUKEE  
TAX KEY: PWC 0957999008

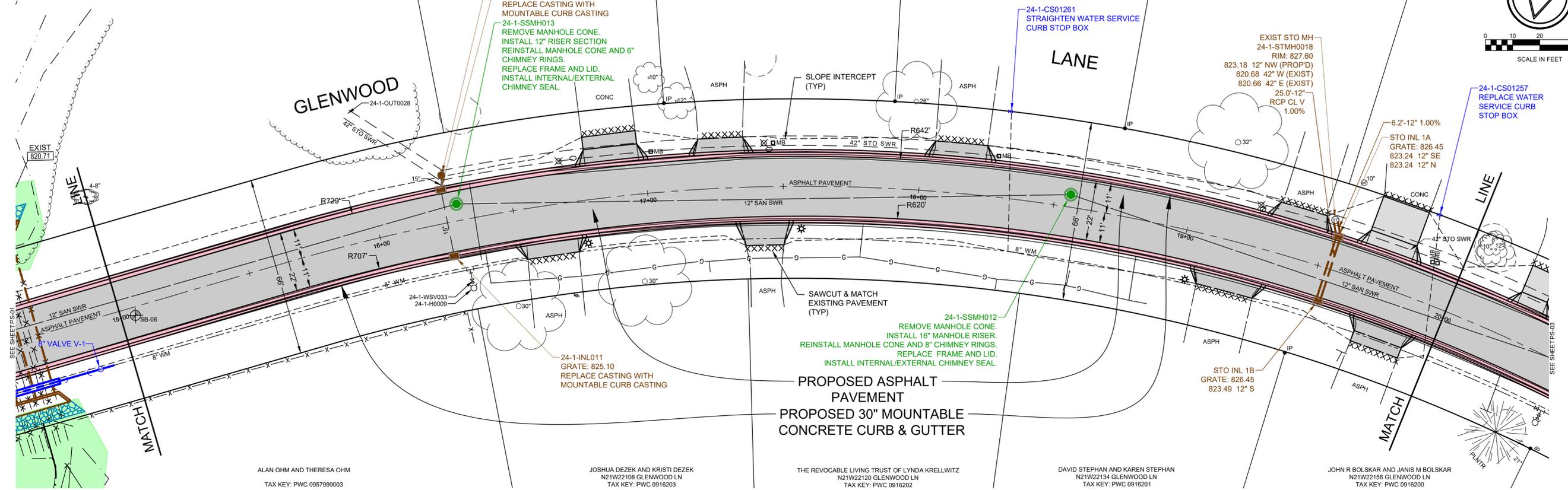
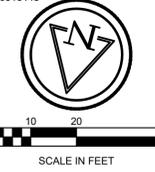
MARK BOOTH AND CHRISTINE BOOTH  
N21W22103 GLENWOOD LN  
TAX KEY: PWC 0916152  
24-1-STMH017  
TUCKPOINT MANHOLE  
24-1-INL-010  
GRATE: 825.10  
REPLACE CASTING WITH  
MOUNTABLE CURB CASTING

PAUL ZAVALA AND NICOLE ZAVALA  
N21W22115 GLENWOOD LN  
TAX KEY: PWC 0916151

AARON DANIEL PAULEY AND LINDSEY MCCOY PAULEY  
N21W22127 GLENWOOD LN  
TAX KEY: PWC 0916150

ROBERT SLAVENS AND K SLAVENS  
N21W22139 GLENWOOD LN  
TAX KEY: PWC 0916149

JON NONDAHL AND P NONDAHL  
N21W22153 GLENWOOD LN  
TAX KEY: PWC 0916148



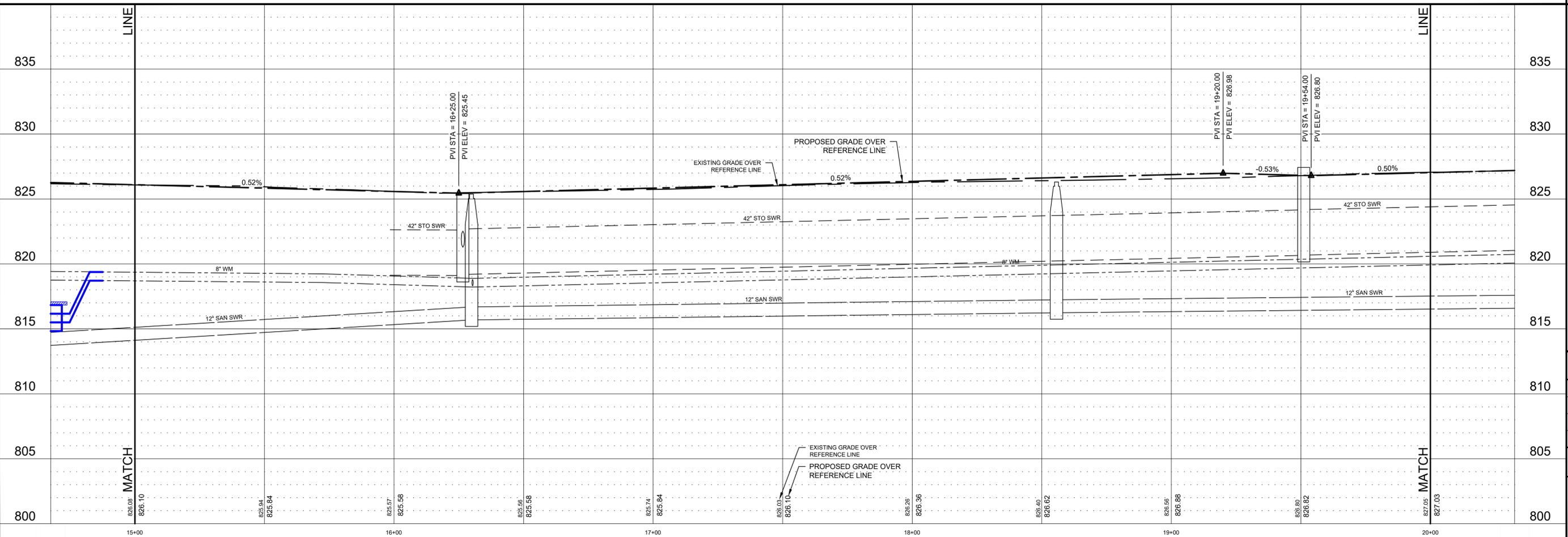
ALAN OHM AND THERESA OHM  
TAX KEY: PWC 0957999003

JOSHUA DEZEK AND KRISTI DEZEK  
N21W22109 GLENWOOD LN  
TAX KEY: PWC 0916203

THE REVOCABLE LIVING TRUST OF LYNDA KRELLWITZ  
N21W22120 GLENWOOD LN  
TAX KEY: PWC 0916202

DAVID STEPHAN AND KAREN STEPHAN  
N21W22134 GLENWOOD LN  
TAX KEY: PWC 0916201

JOHN R BOLSCHAR AND JANIS M BOLSCHAR  
N21W22156 GLENWOOD LN  
TAX KEY: PWC 0916200



**GLENWOOD LANE RECONSTRUCTION  
PROPOSED PAVEMENT & STORM SEWER**  
IN: GLENWOOD LANE  
FROM: 500' WEST OF SPRINDALE ROAD/CTH "SR"  
TO: 1000' WEST OF SPRINDALE ROAD/CTH "SR"  
CITY OF PEWAUKEE  
WAUKESHA COUNTY, WISCONSIN

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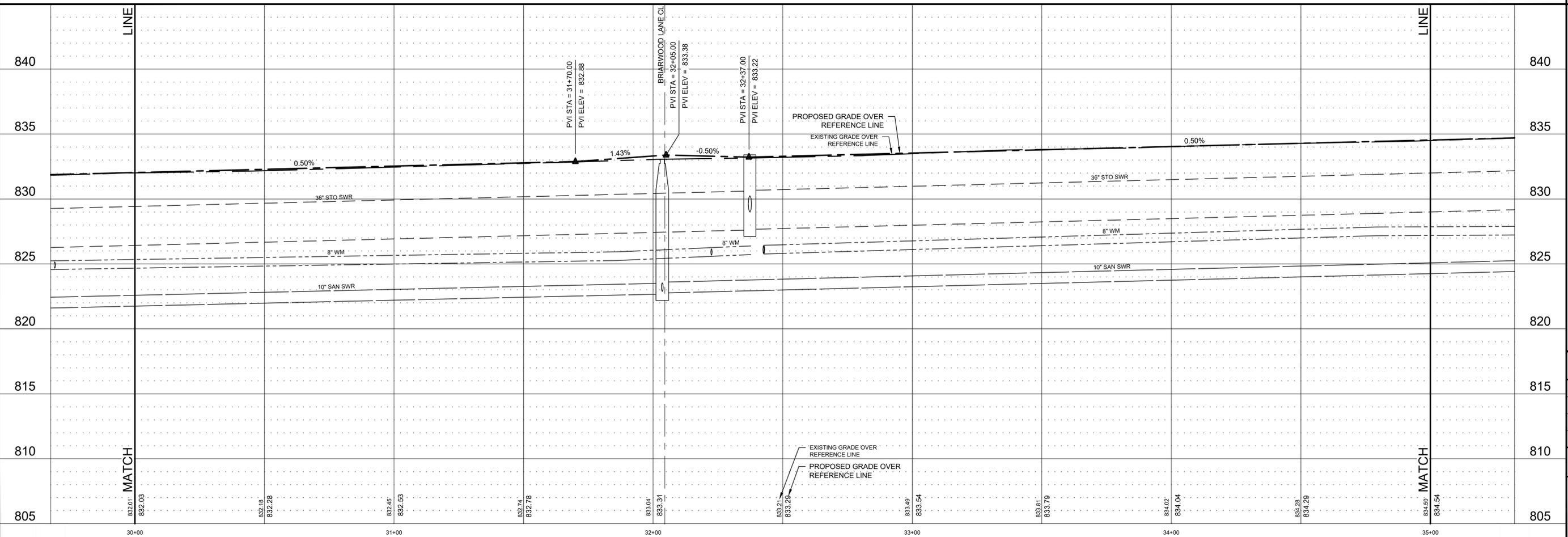
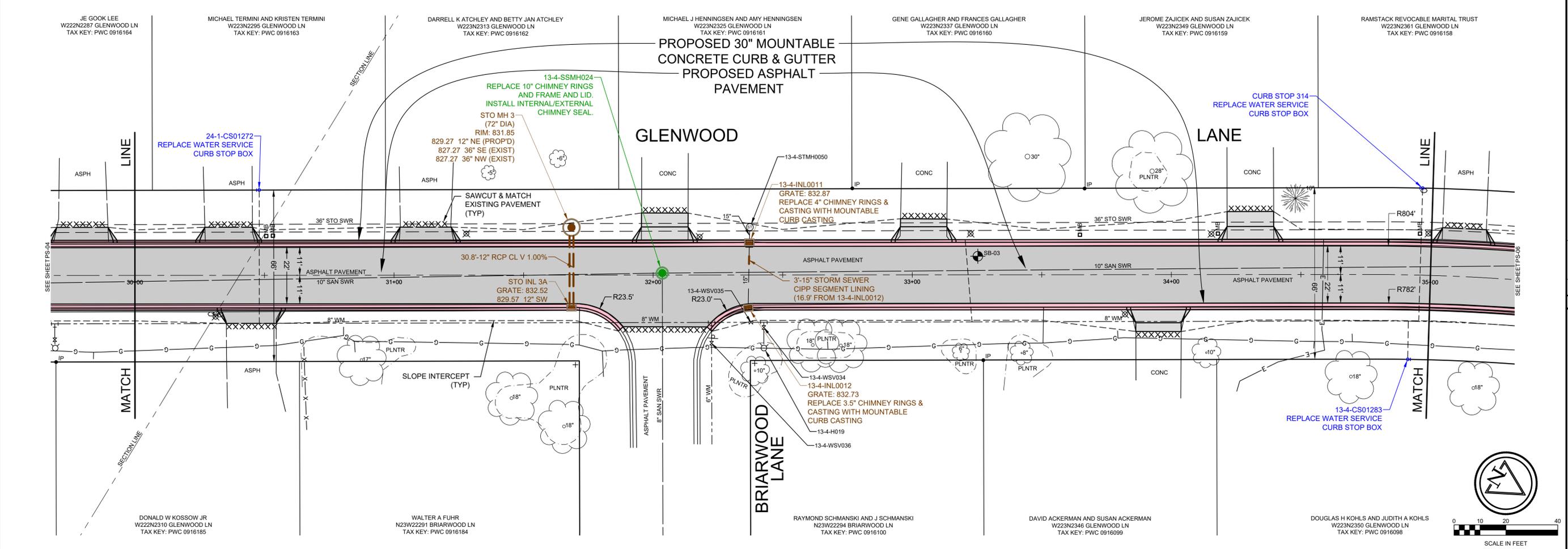
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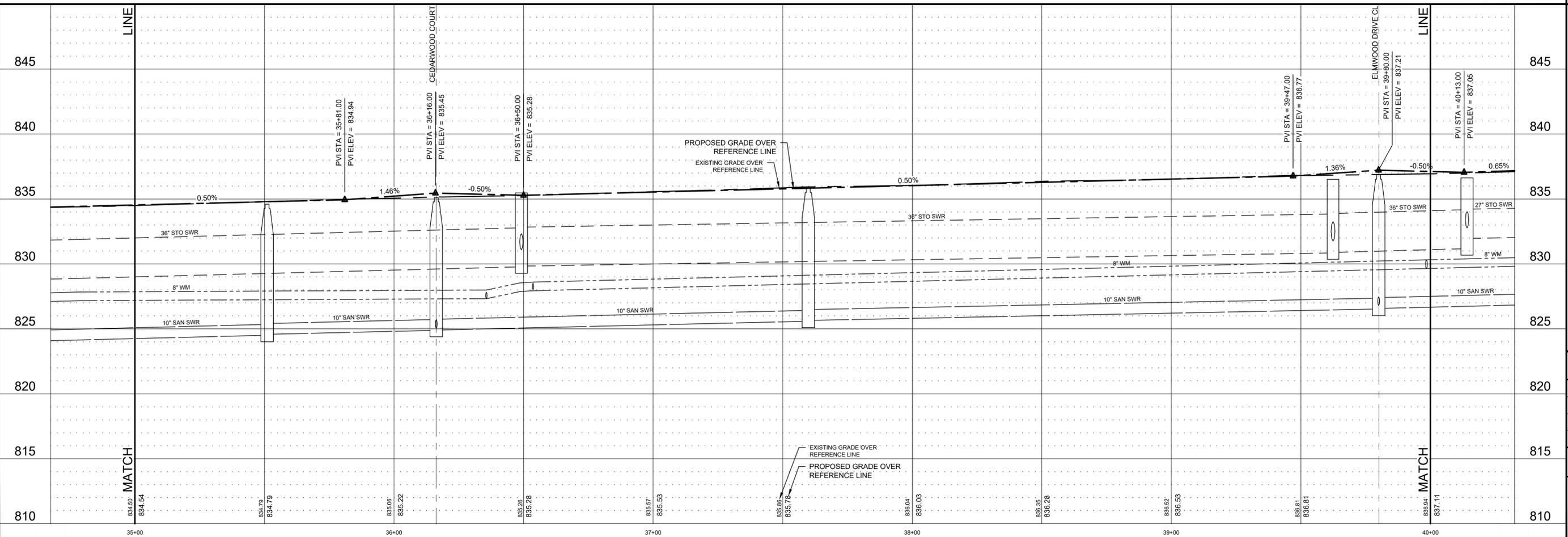
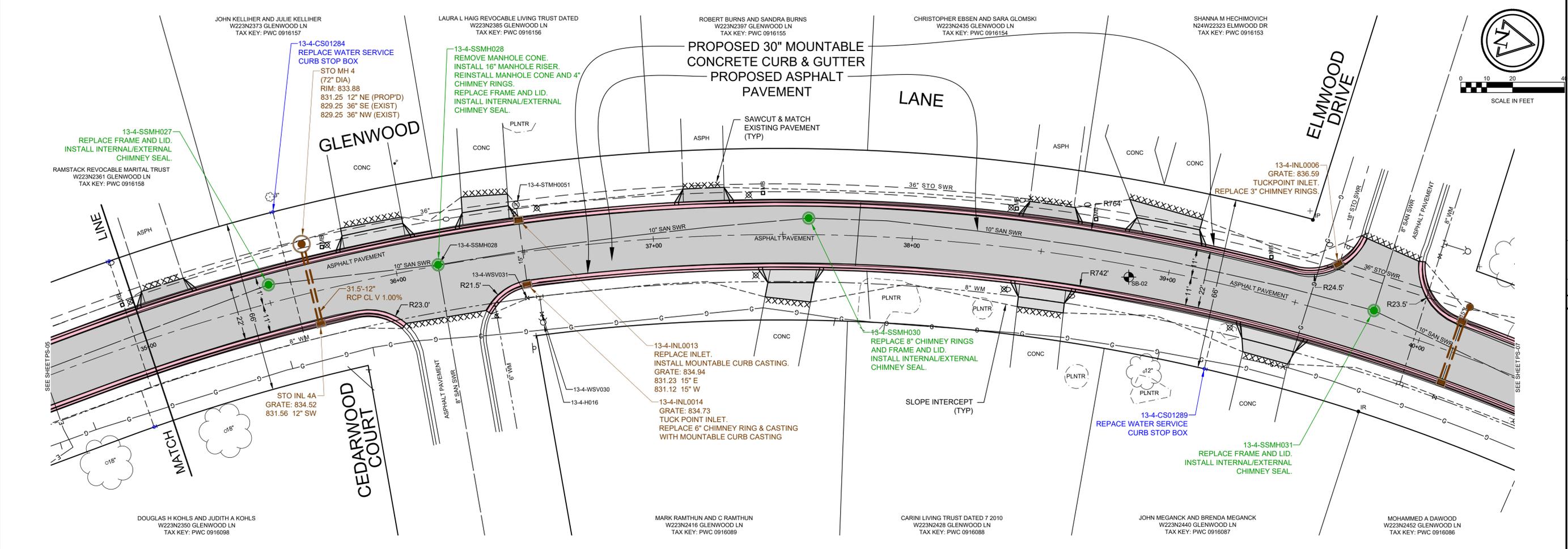
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**GLENWOOD LANE RECONSTRUCTION**  
**PROPOSED PAVEMENT & STORM SEWER**  
 IN: GLENWOOD LANE  
 FROM: 150' SOUTHEAST OF BRIARWOOD LANE  
 TO: 50' SOUTHEAST OF CEDARWOOD COURT  
 CITY OF PEWAUKEE  
 WAUKESHA COUNTY, WISCONSIN

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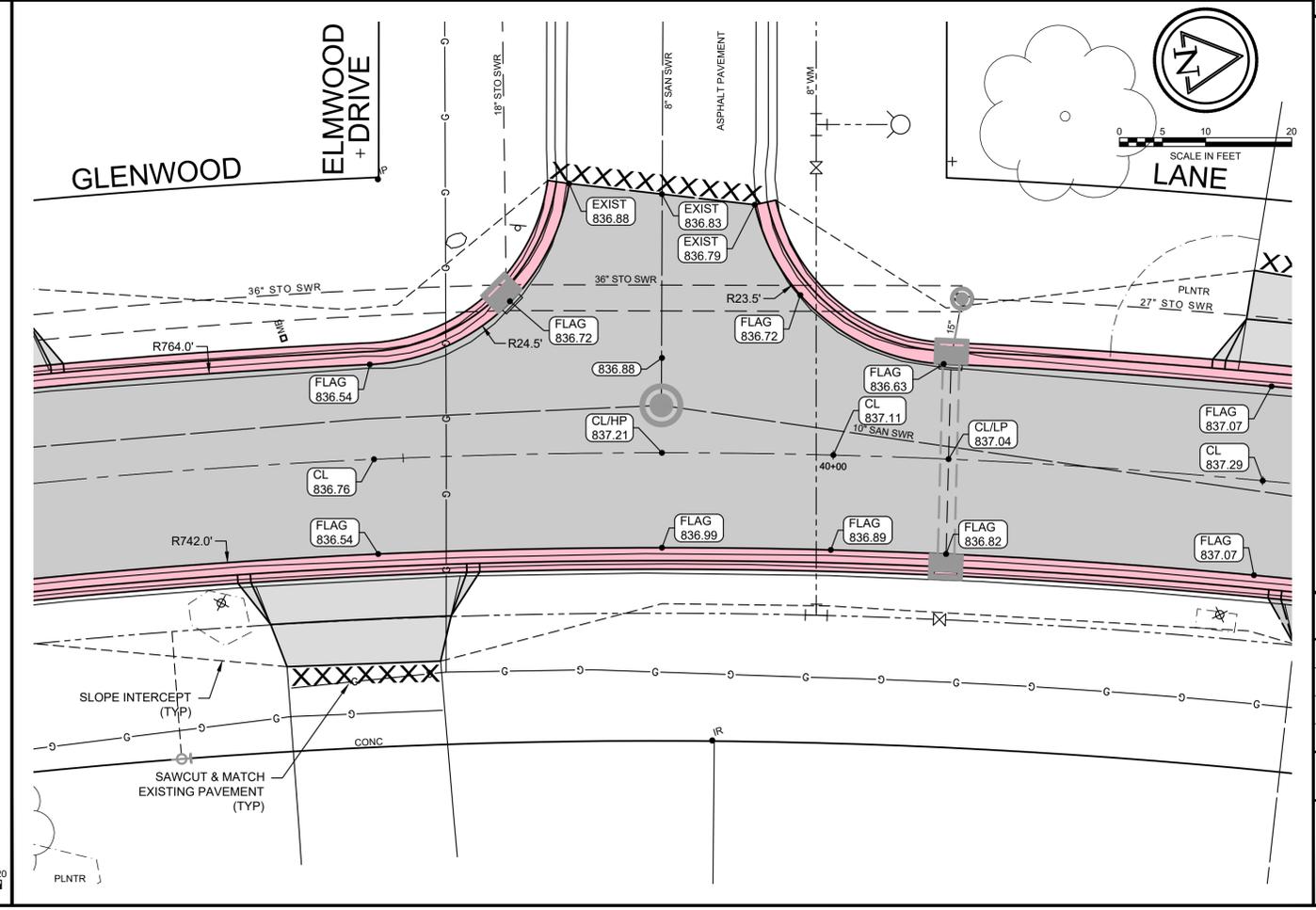
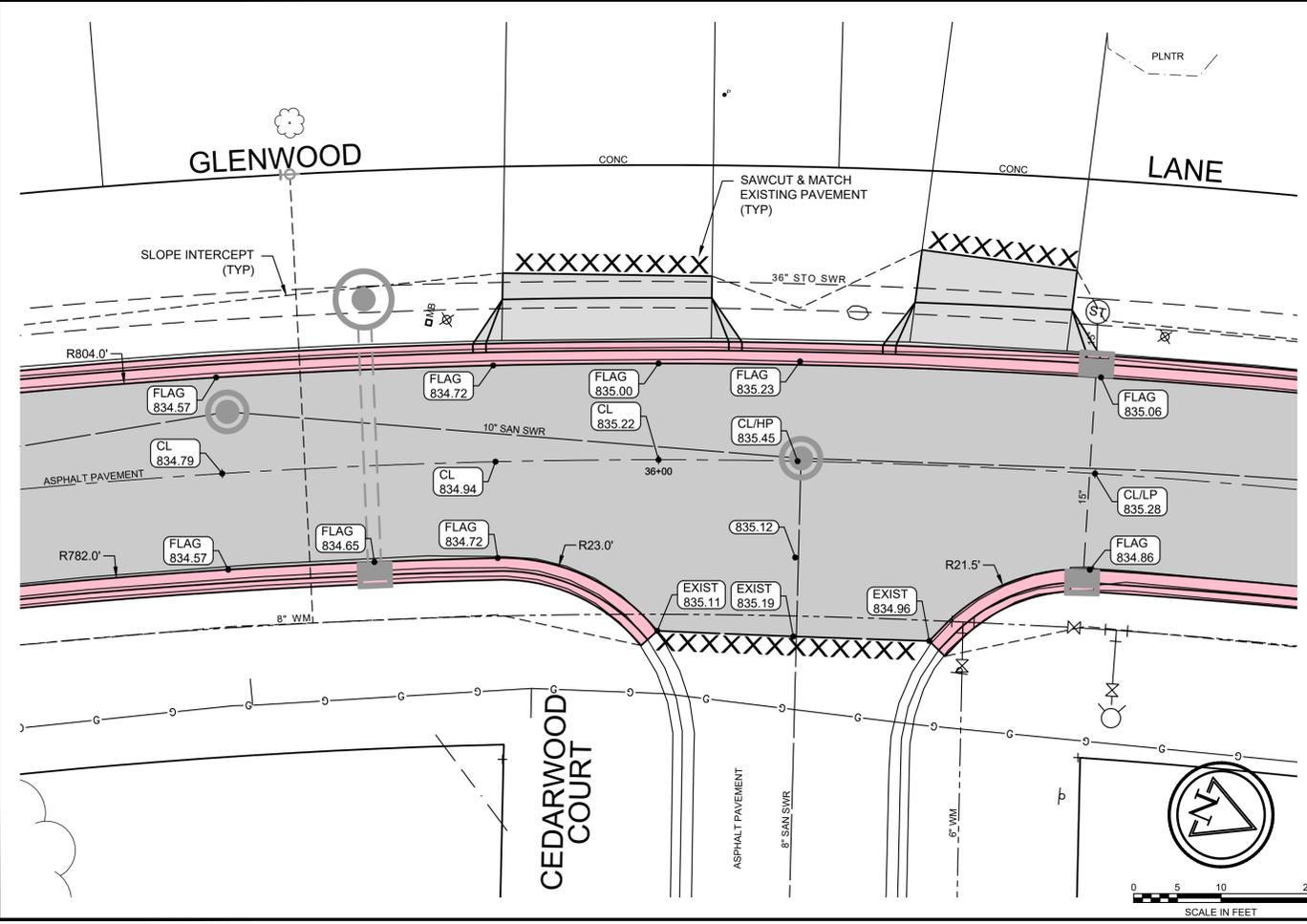
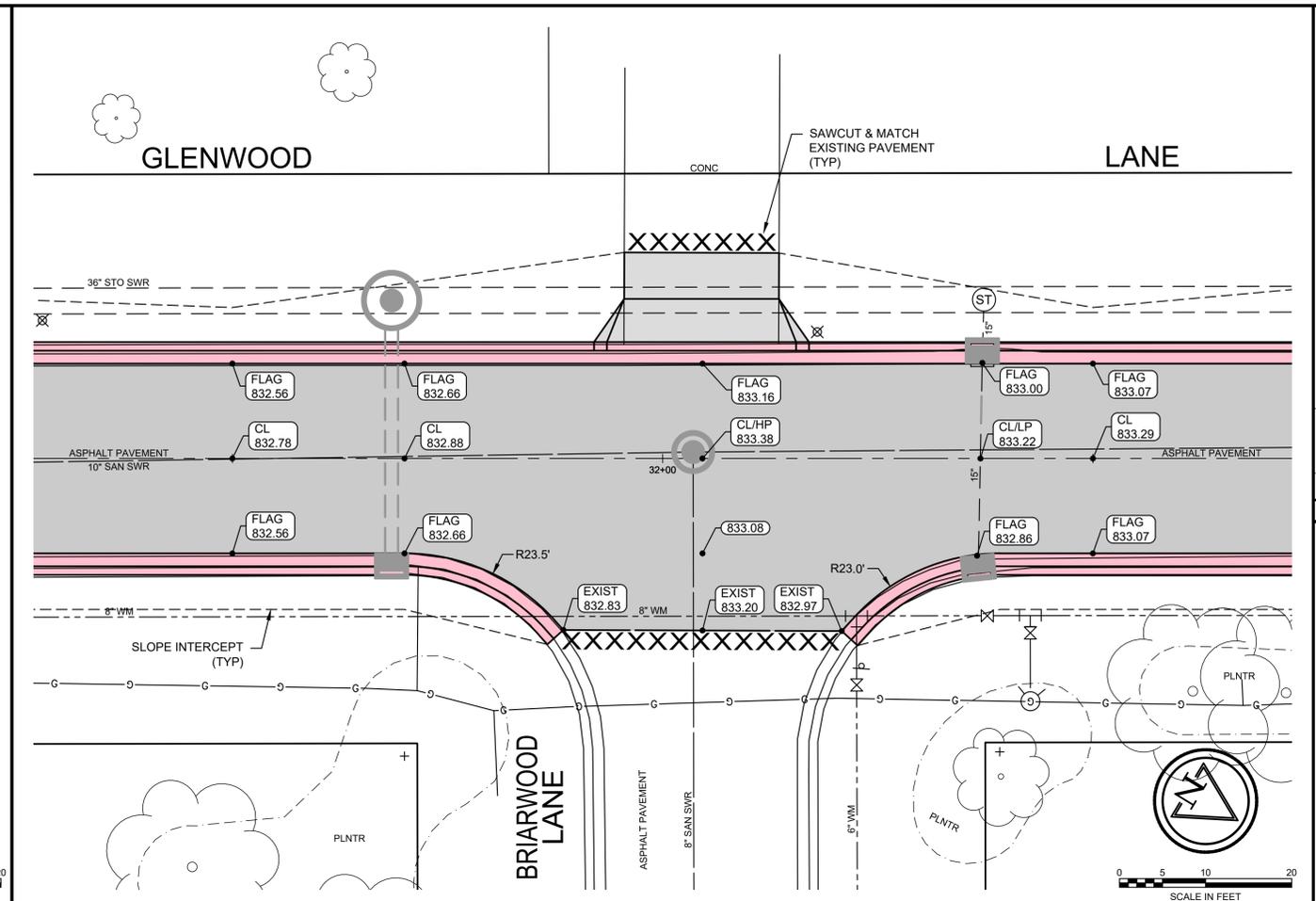
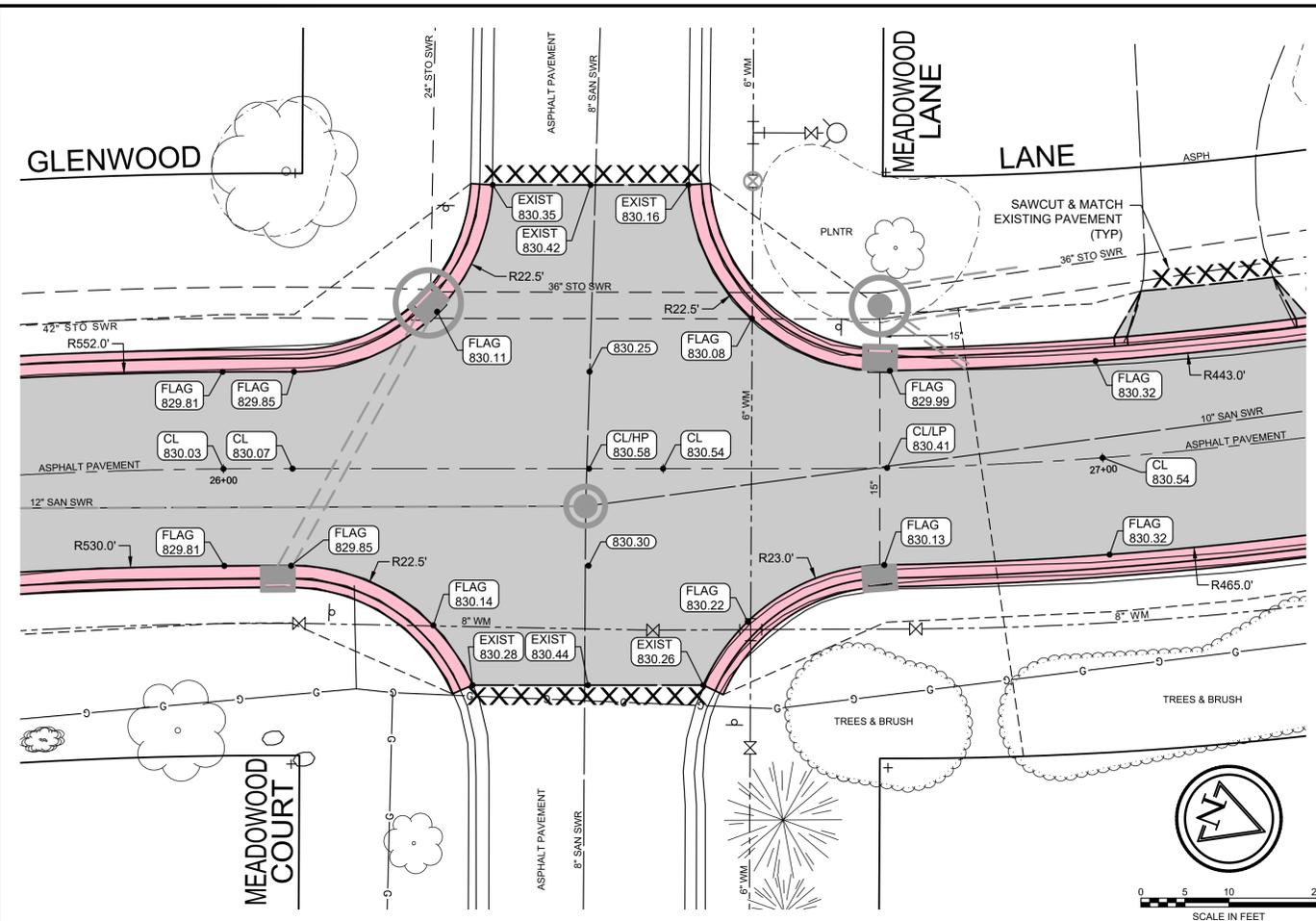
**GLENWOOD LANE RECONSTRUCTION  
 PROPOSED PAVEMENT & STORM SEWER**  
 IN: GLENWOOD LANE  
 FROM: 50' SOUTHEAST OF CEDARWOOD COURT  
 TO: ELMWOOD DRIVE  
 CITY OF PEWAUKEE  
 WAUKESHA COUNTY, WISCONSIN

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**GLENWOOD LANE RECONSTRUCTION**  
 INTERSECTION DETAIL - GLENWOOD LANE  
 CITY OF PEWAUKEE  
 WAUKESHA COUNTY, WISCONSIN

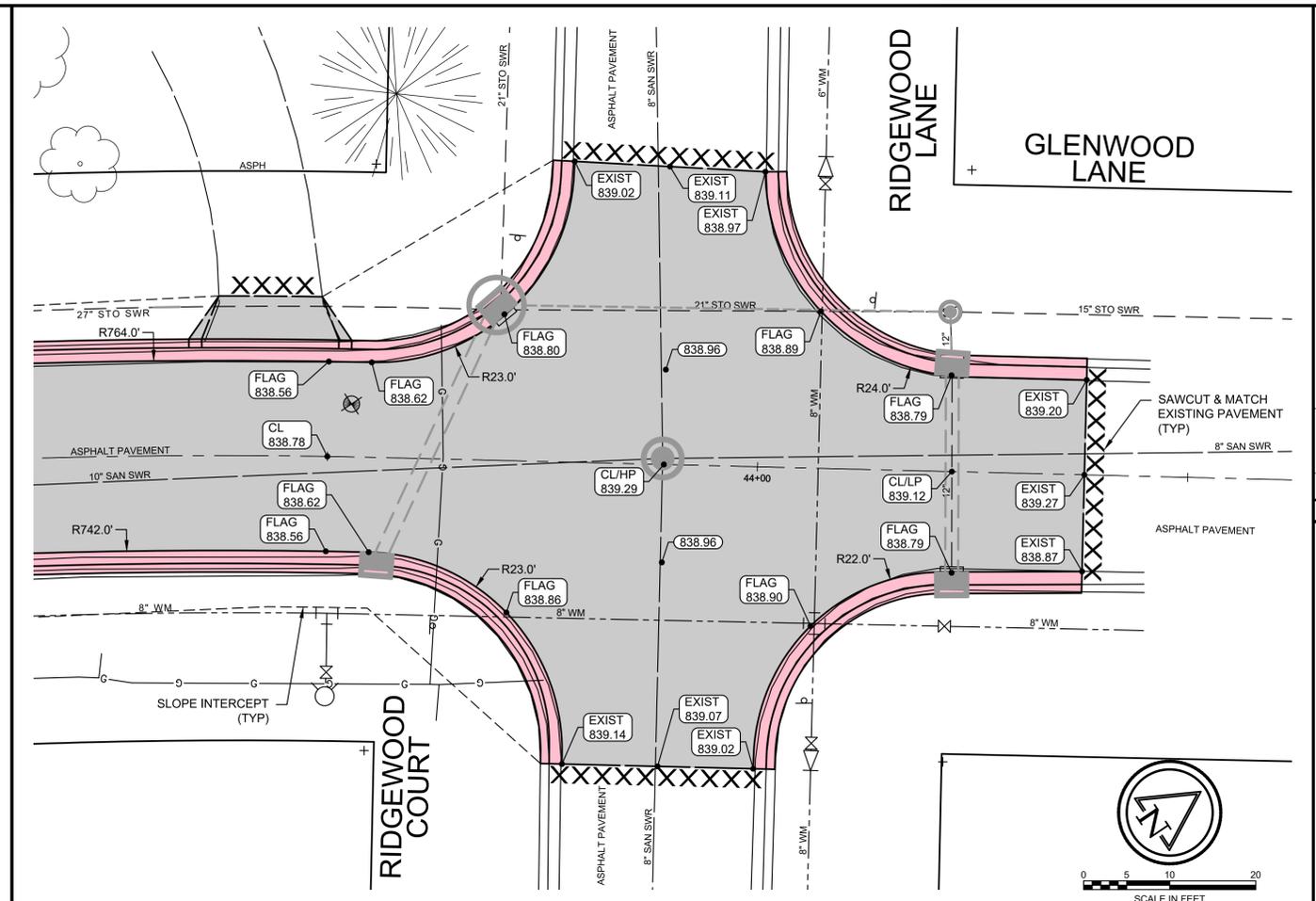
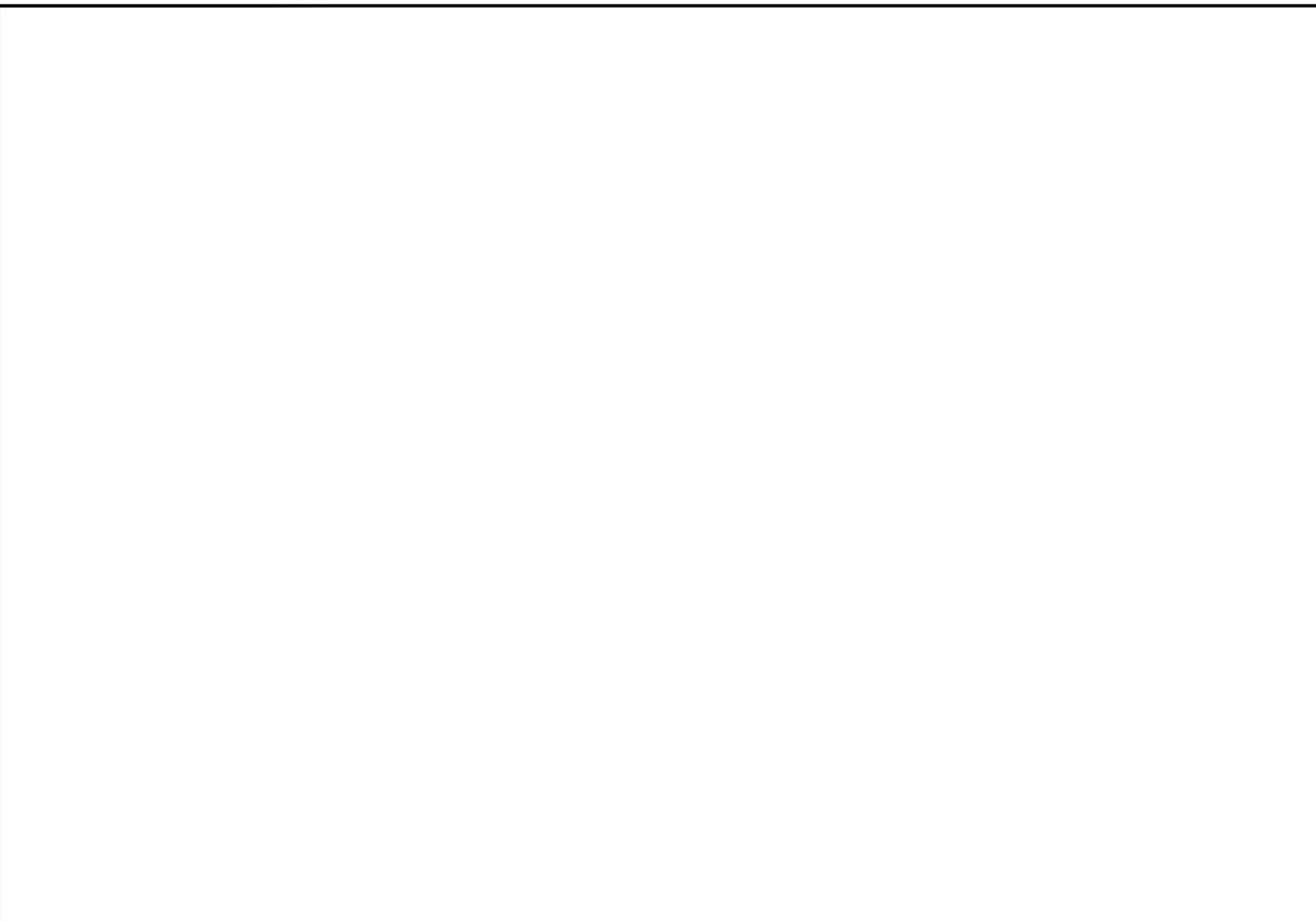
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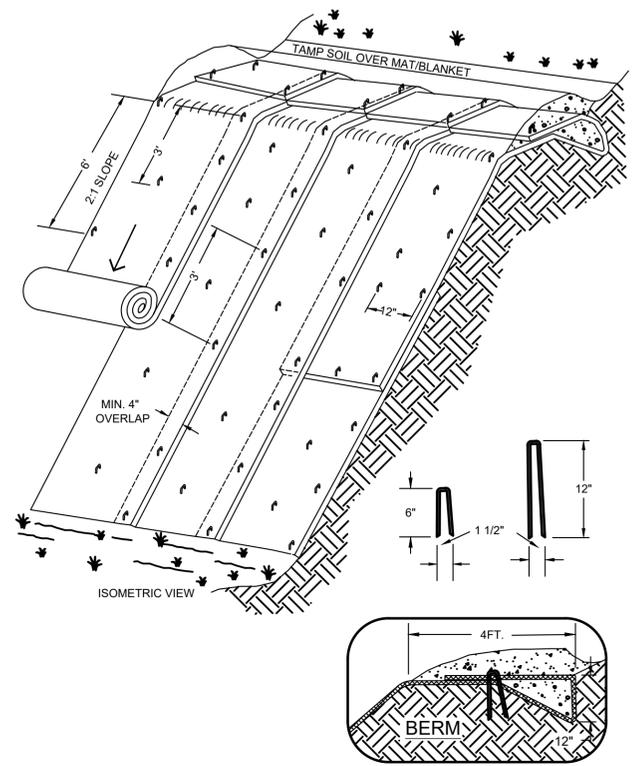
**GLENWOOD LANE RECONSTRUCTION**  
**INTESECTION DETAIL - GLENWOOD LANE**  
**CITY OF PEWAUKEE**  
**WAUKESHA COUNTY, WISCONSIN**

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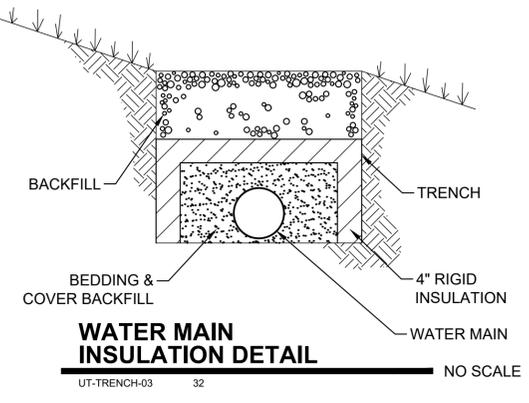
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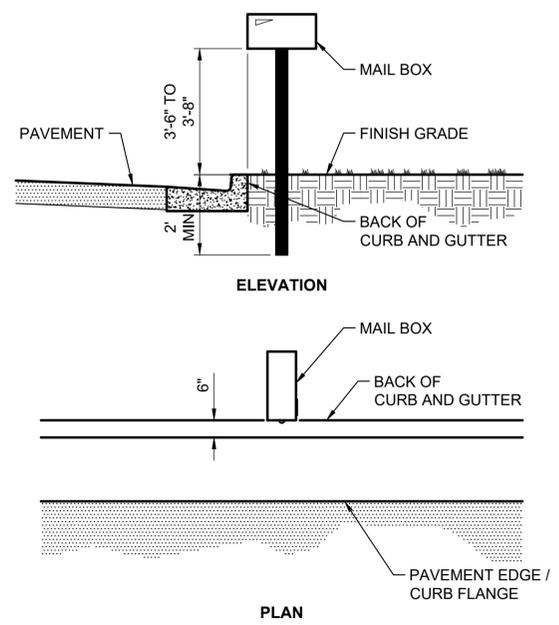
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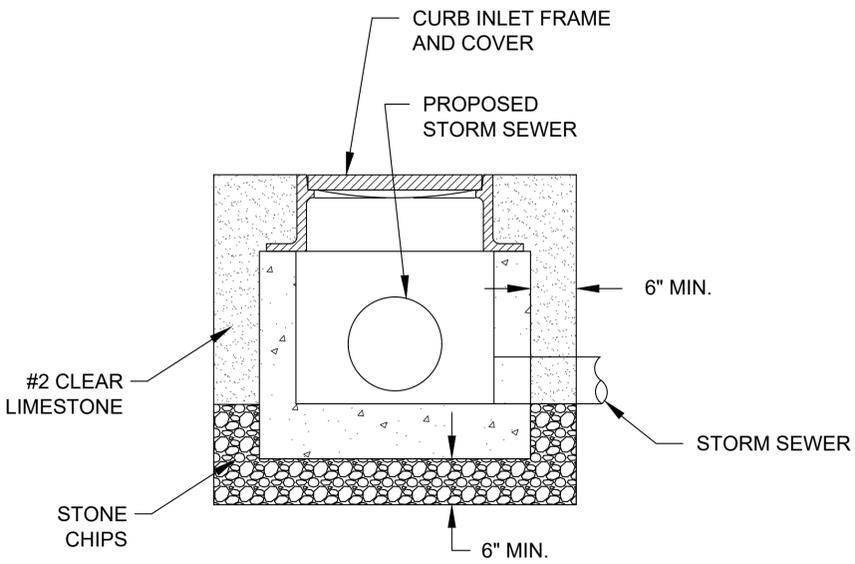
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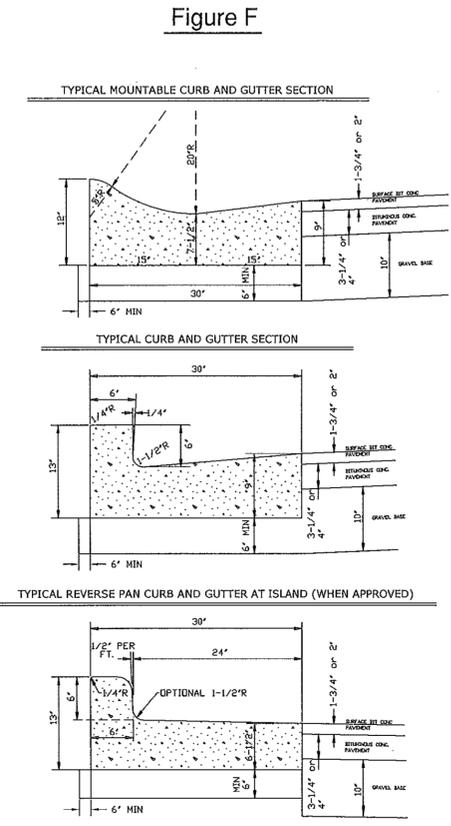
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 UT-TRENCH-03 32



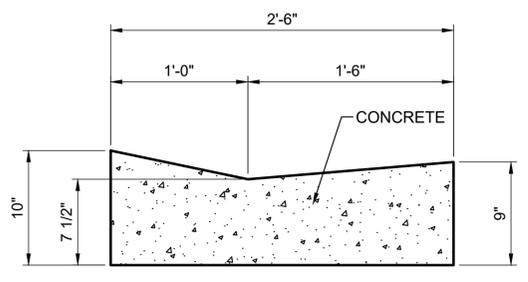
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 SS-POST-07 32



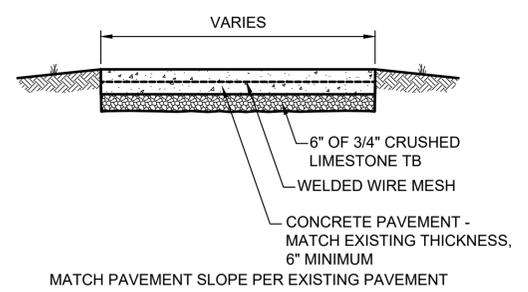
**STORM INLET** NO SCALE  
 STO-CB-11 1



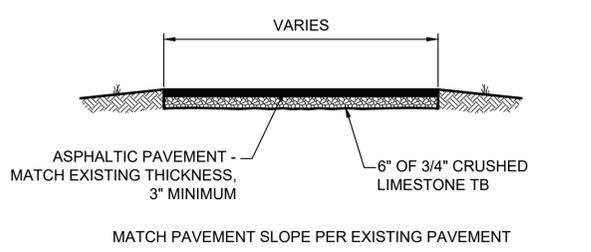
**CITY OF PEWAUKEE**  
 SCALE: 1"=1' SEPTEMBER 2016 DRAWN BY: JJF



**DRIVEWAY CURB & GUTTER** NO SCALE  
 PV-CURB-09 1



**COMMON CONCRETE DRIVEWAY CROSS-SECTION** NO SCALE  
 PV-ASPH-05-CONC 48



**COMMON ASPHALT DRIVEWAY CROSS-SECTION** NO SCALE  
 PV-ASPH-05-ASPH 48

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**GLENWOOD LANE RECONSTRUCTION**  
**CONSTRUCTION DETAILS**  
**CITY OF PEWAUKEE**  
**WAUKESHA COUNTY, WISCONSIN**

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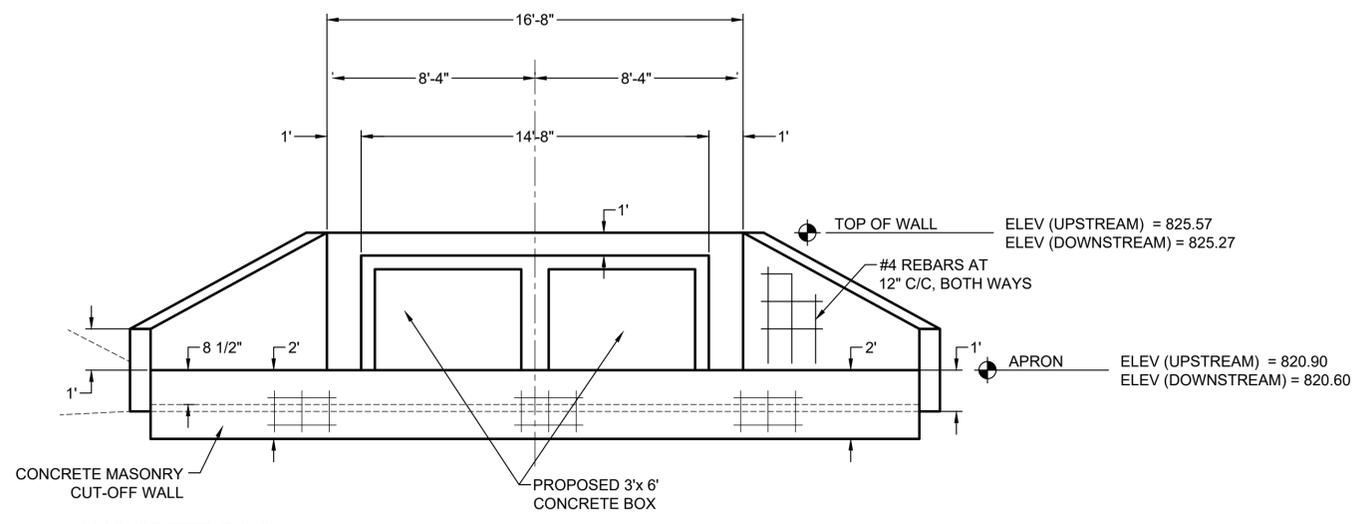
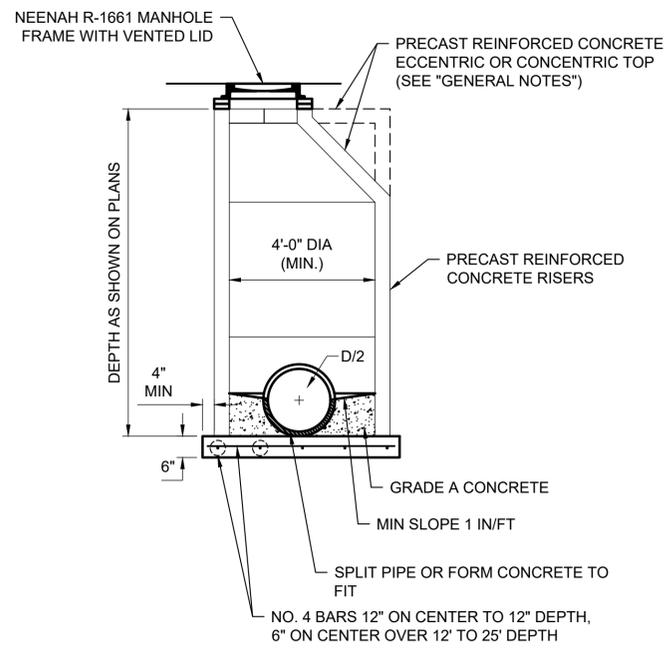
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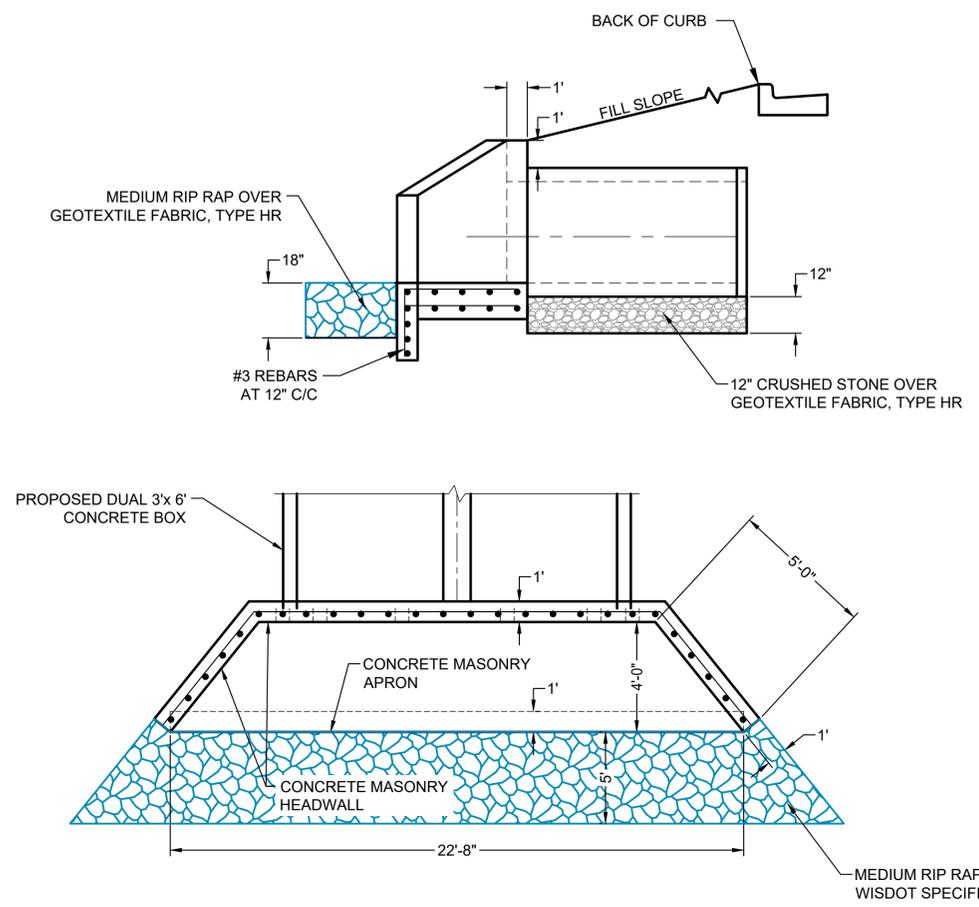
**STORM MANHOLE** NO SCALE

STO-MH-07 1



**CONCRETE BOX  
CULVERT ENDWALL** NO SCALE

Cgdt-End Wall Detail-Box Culvert ST-04 5



**NOTES:**  
 DETAILS OF CONSTRUCTION MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.  
 FILL SLOPES SHALL BE SHAPED TO MEET TOP OF THE WINGWALLS  
 ALL STEEL REINFORCEMENT AND WELDED STEEL WIRE FABRIC SHALL BE EMBEDDED 2-INCHES CLEAR UNLESS OTHERWISE NOTED.

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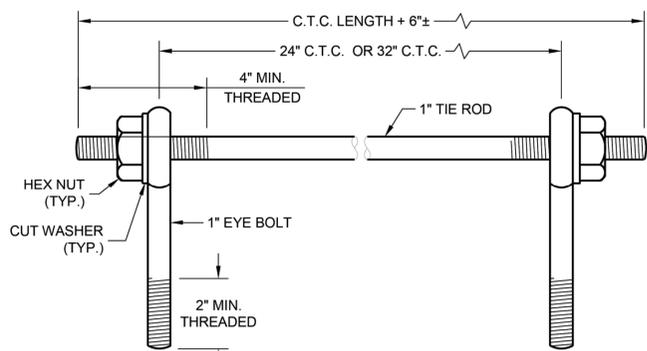
GLENWOOD LANE RECONSTRUCTION  
 CONSTRUCTION DETAILS  
 CITY OF PEWAUKEE  
 WAUKESHA COUNTY, WISCONSIN

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**DT-03**

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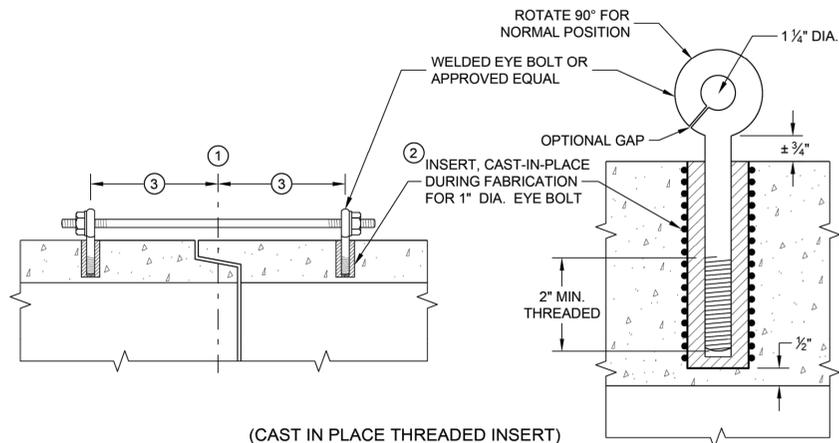


# SDD 08F04 Joint Ties for Concrete Pipe and Concrete Collar Detail



**EYE BOLTS AND TIE ROD**

**EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)**



(CAST IN PLACE THREADED INSERT)  
**LONGITUDINAL SECTIONS**

**GENERAL NOTES**

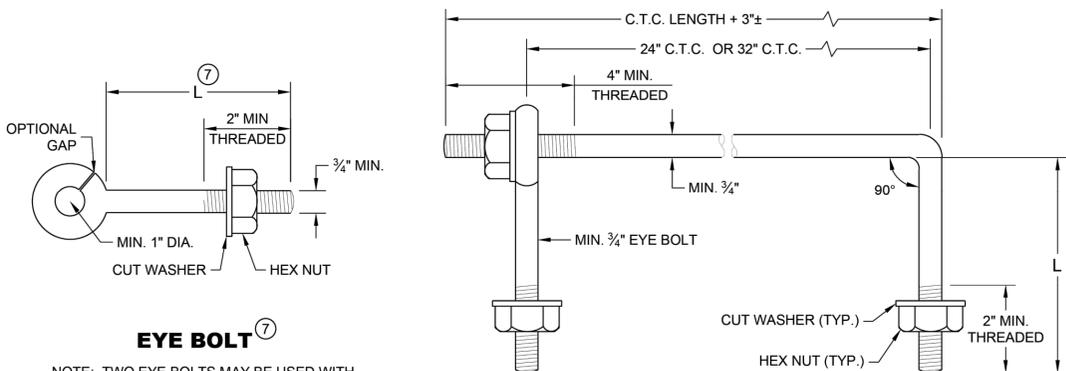
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

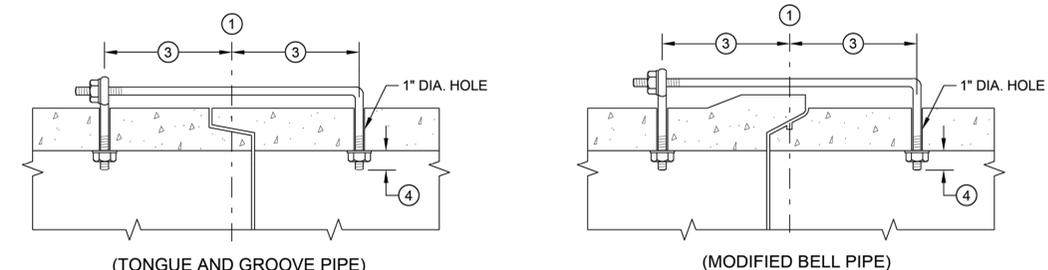
- ① CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- ⑦ EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.



**EYE BOLT**

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" OR 38" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.

**EYE BOLT AND TIE ROD**



(TONGUE AND GROOVE PIPE)

(MODIFIED BELL PIPE)

**LONGITUDINAL SECTION**

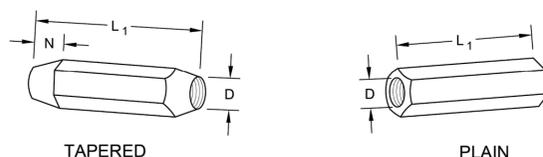
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

**EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)**

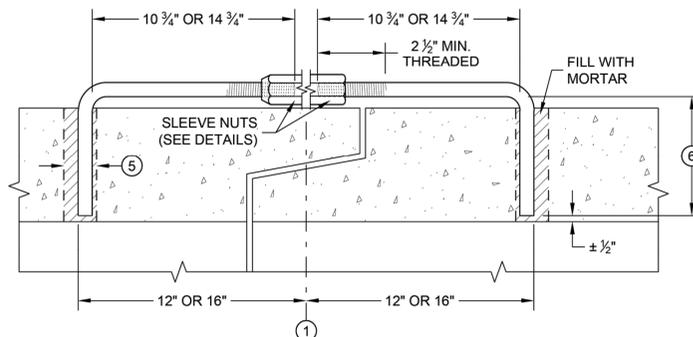
**ADJUSTABLE TIE ROD TABLE**

PIPE DIAMETER	TIE ROD DIAMETER	D	L <sub>1</sub>	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	17/16

DIMENSIONS SHOWN ARE IN INCHES

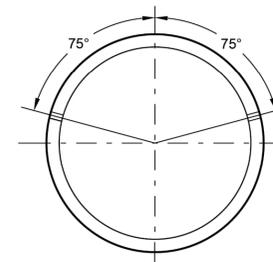


**RIGHT AND LEFT THREADS SLEEVE NUTS**



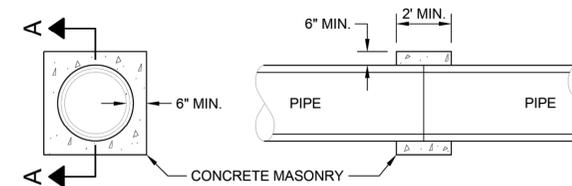
**LONGITUDINAL SECTION**

**ADJUSTABLE TIE ROD (ALTERNATE NO. 3)**



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

**TRANSVERSE SECTION**



**SECTION A - A  
 CONCRETE COLLAR DETAIL**

**JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL**

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION

APPROVED  
 November 2021 /S/ Rodney Taylor  
 DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

**SDD 08F04 - 08**

**SDD 08F04 - 08**

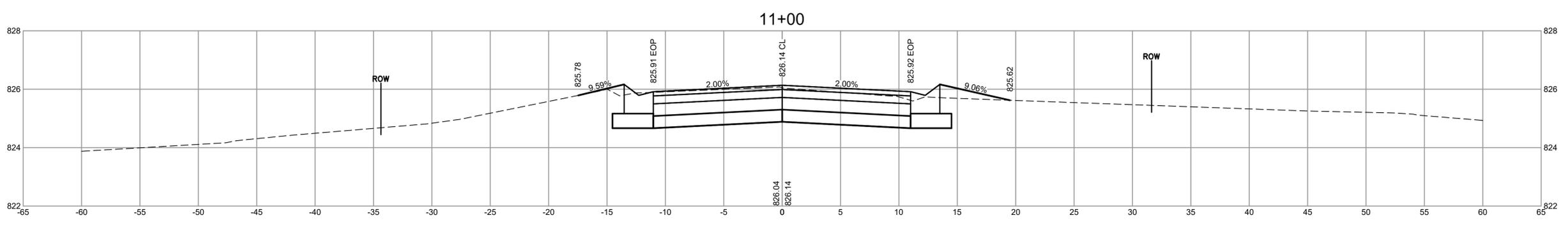
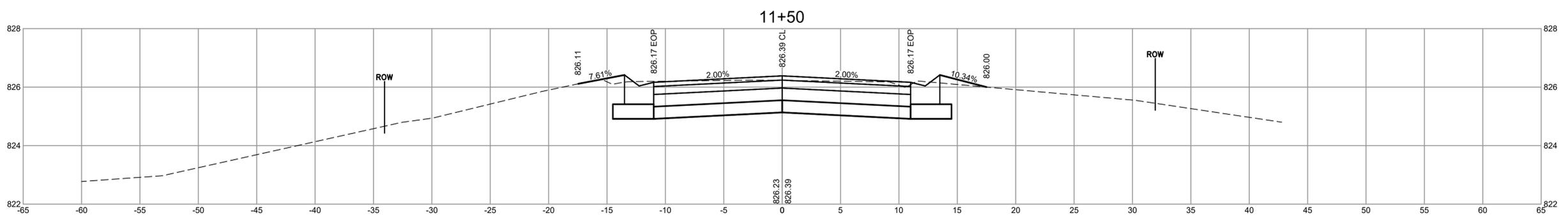
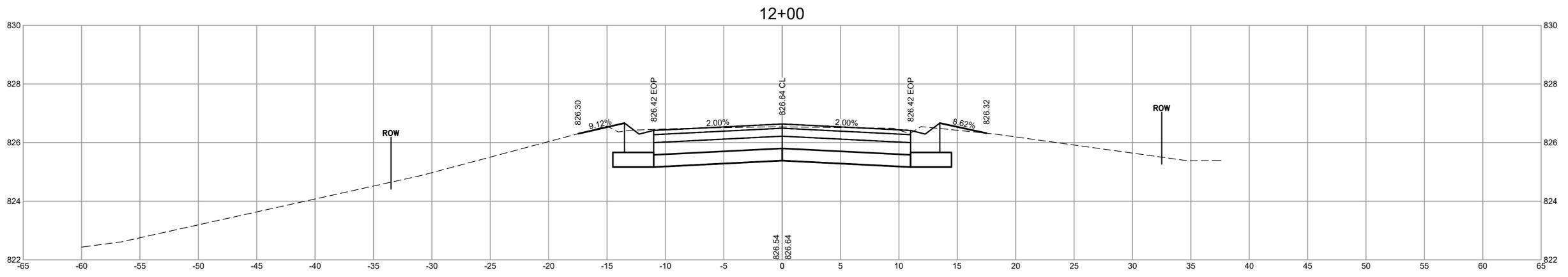
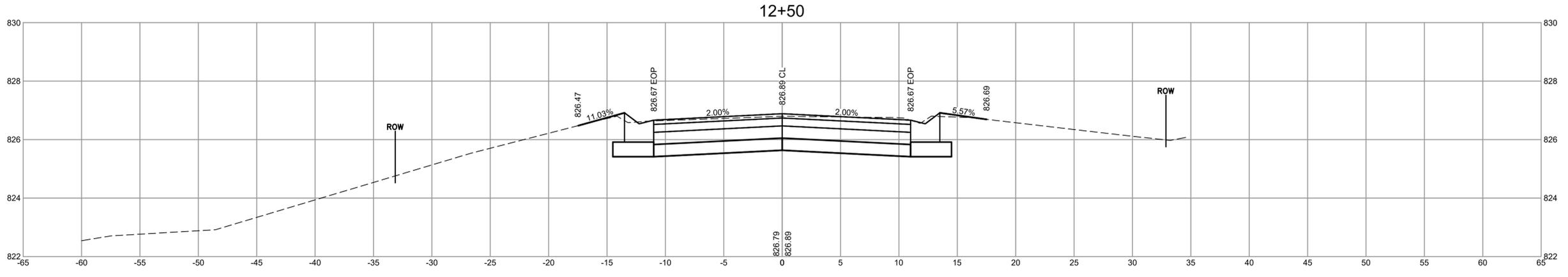
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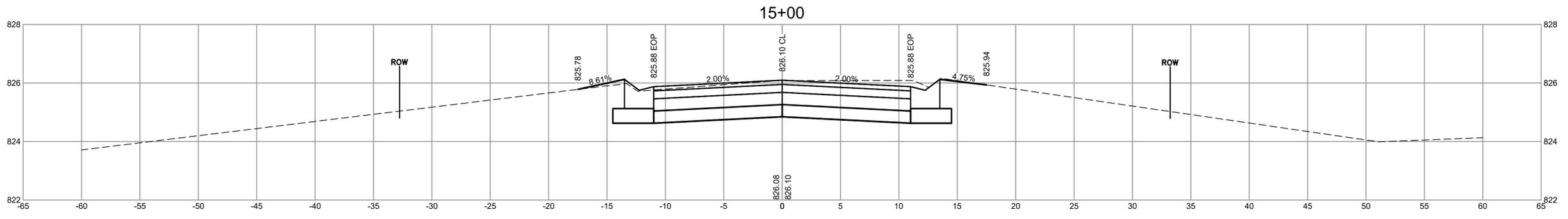
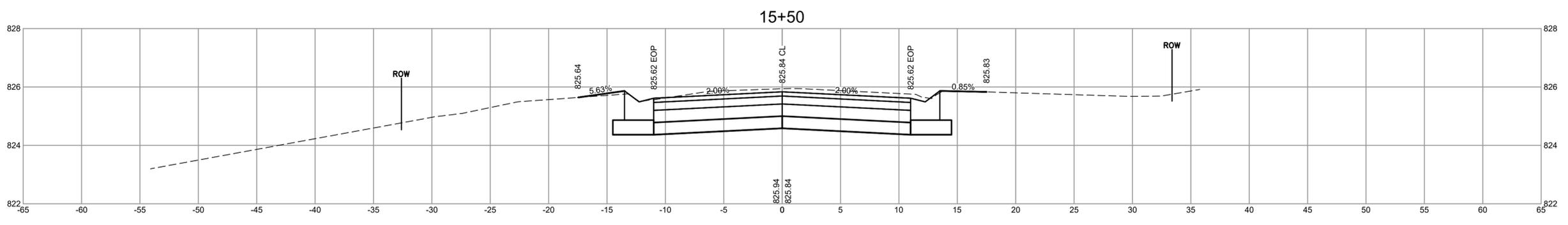
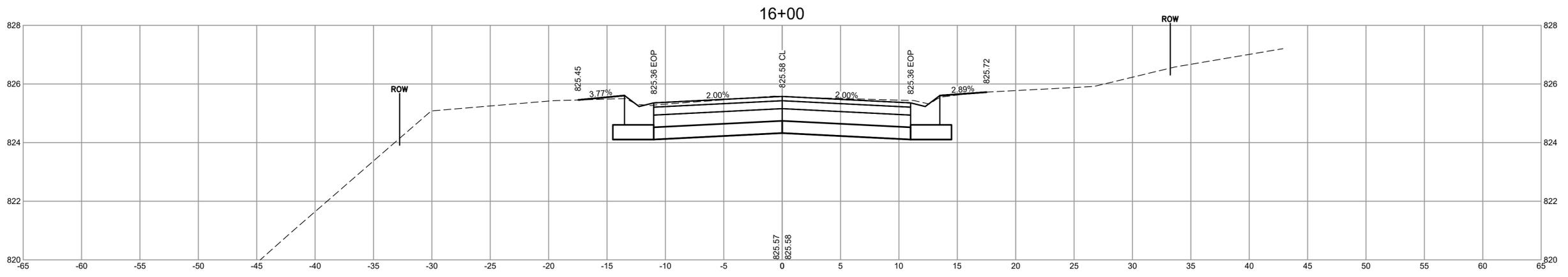
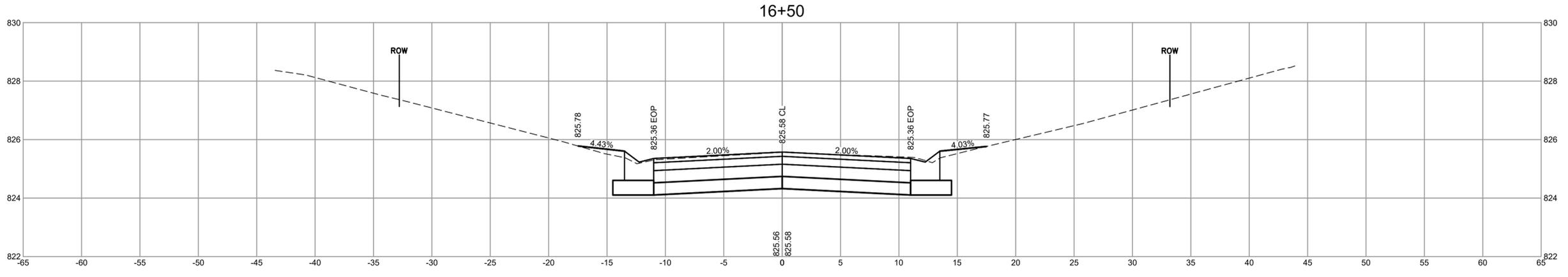
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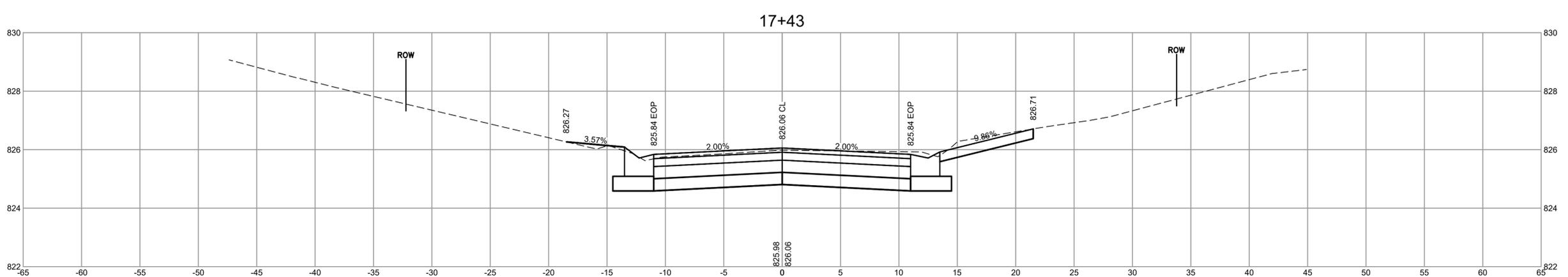
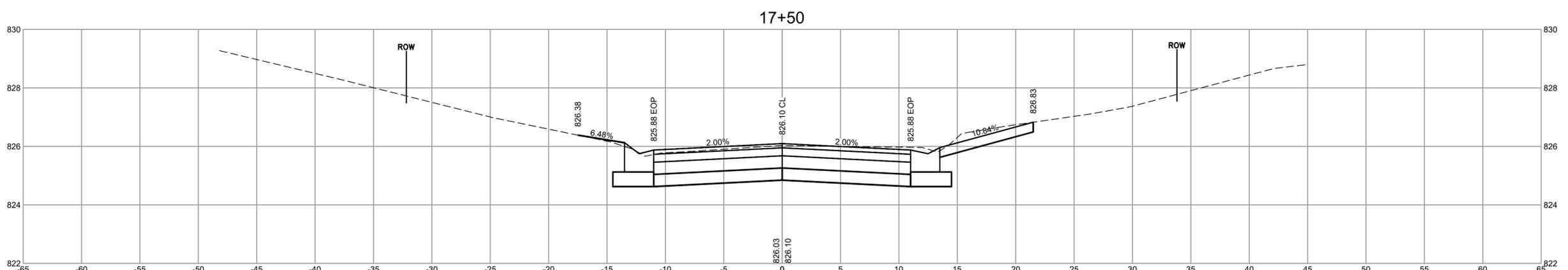
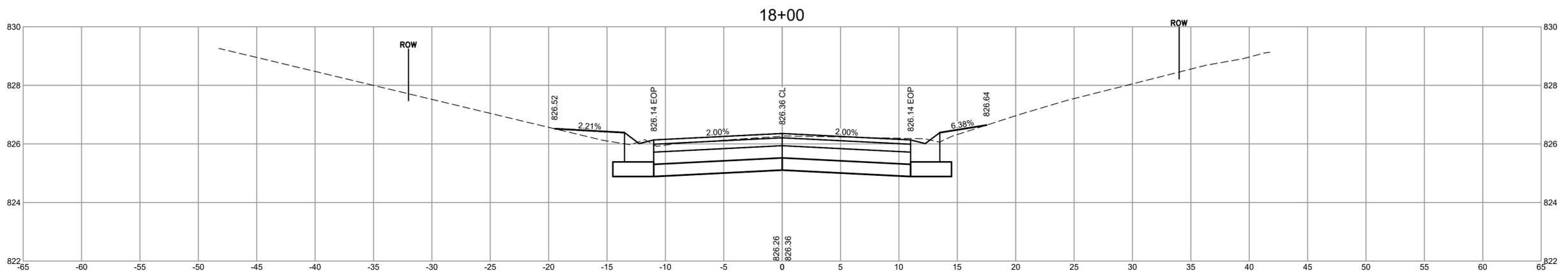
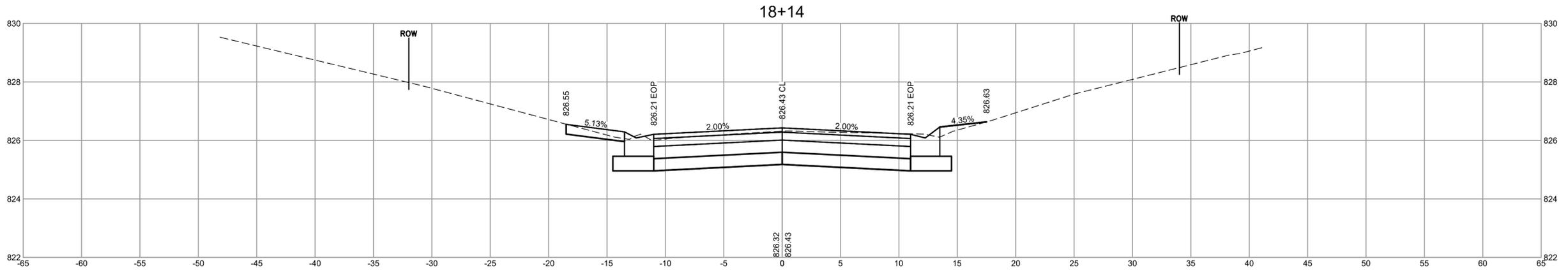
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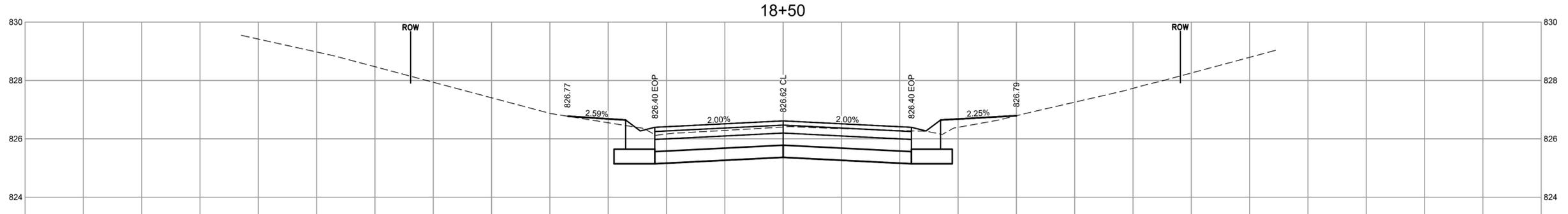
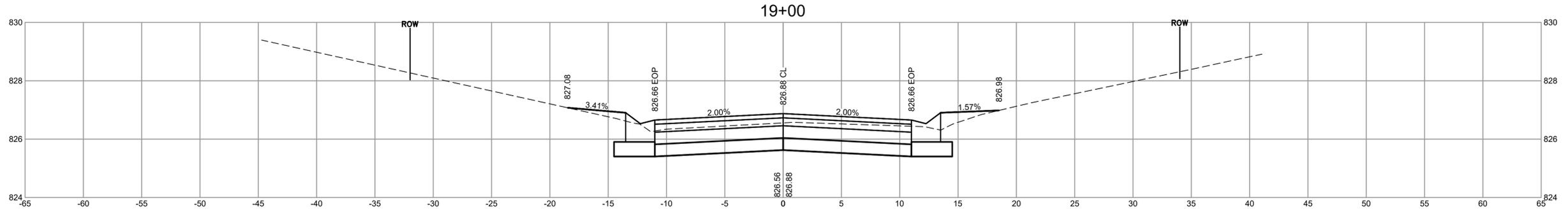
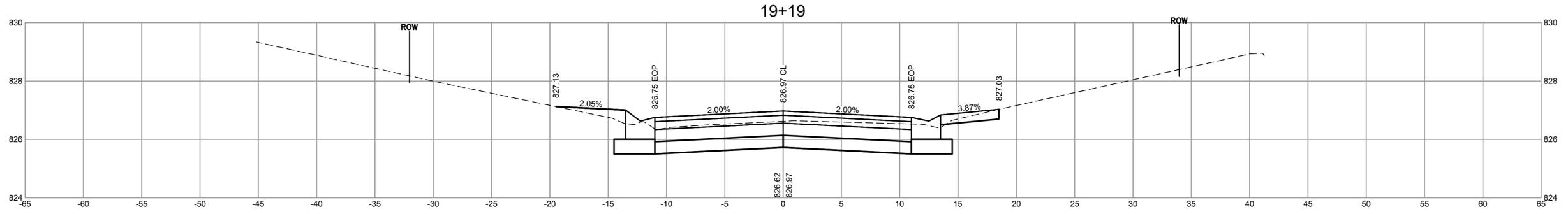
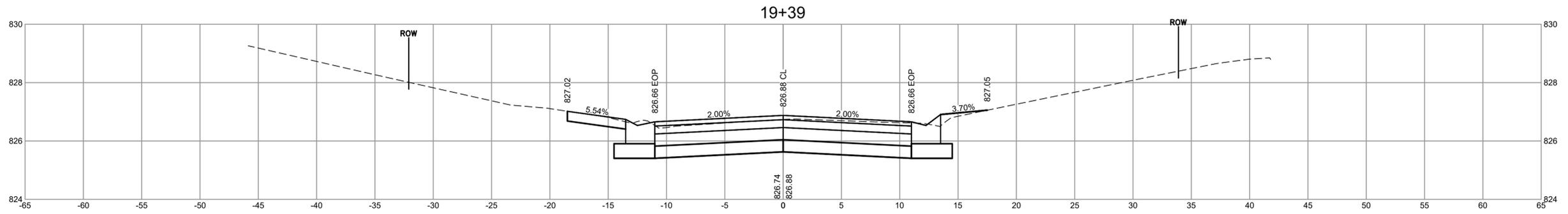
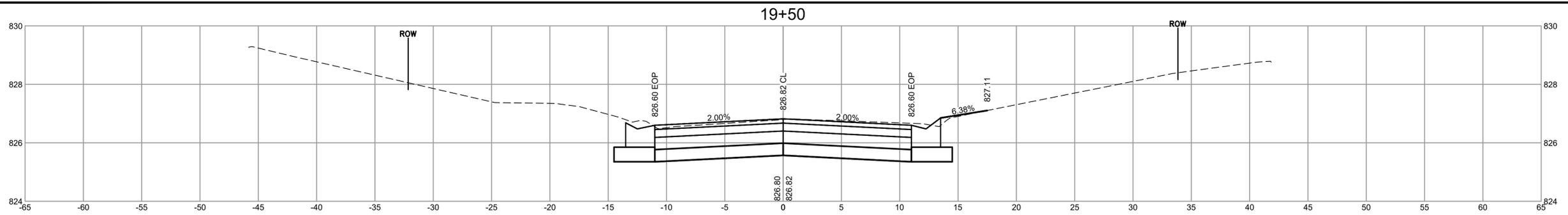
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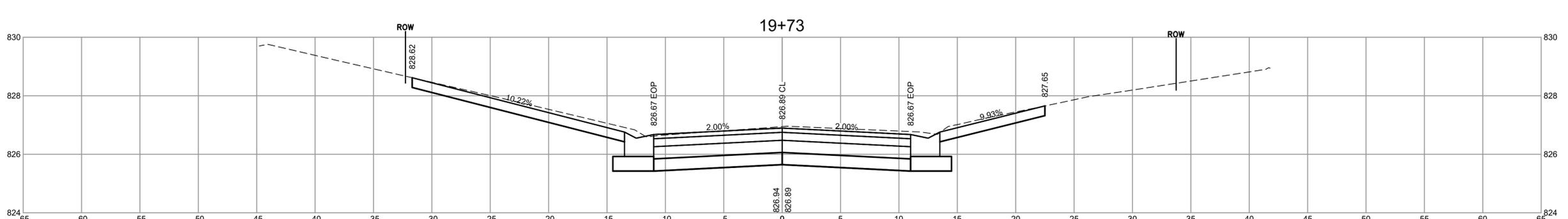
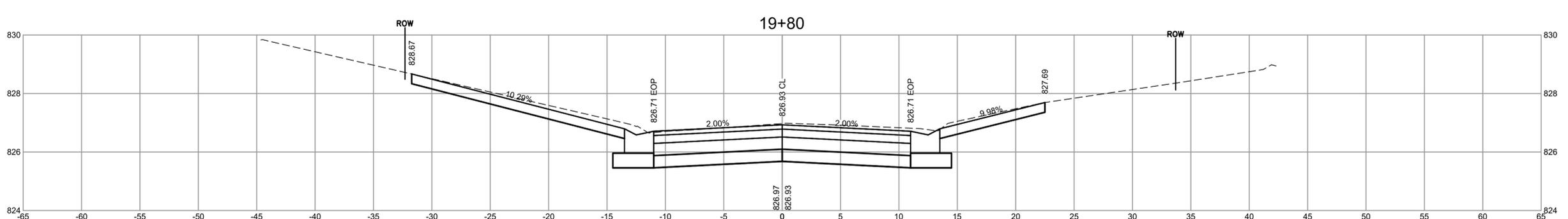
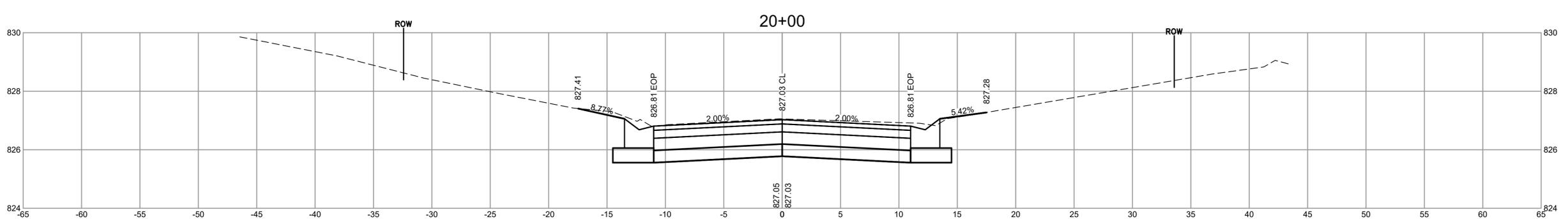
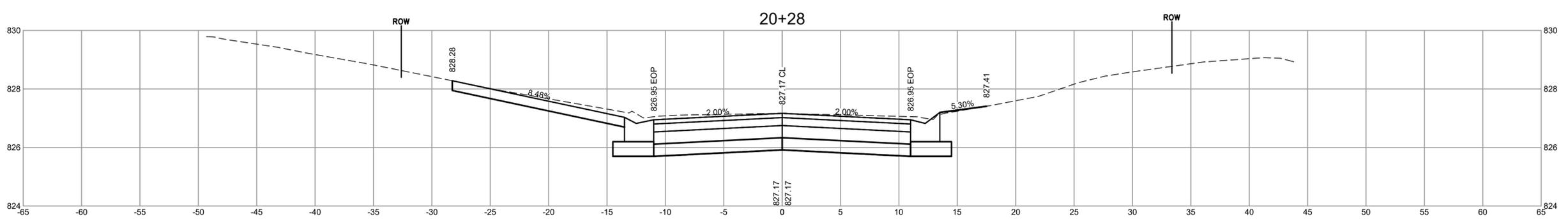
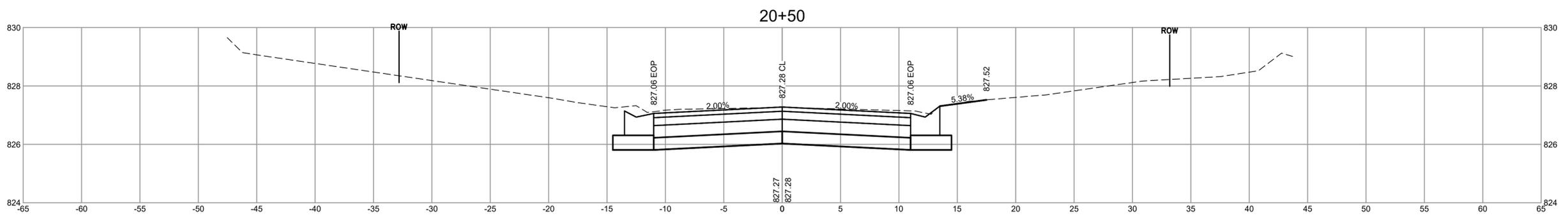
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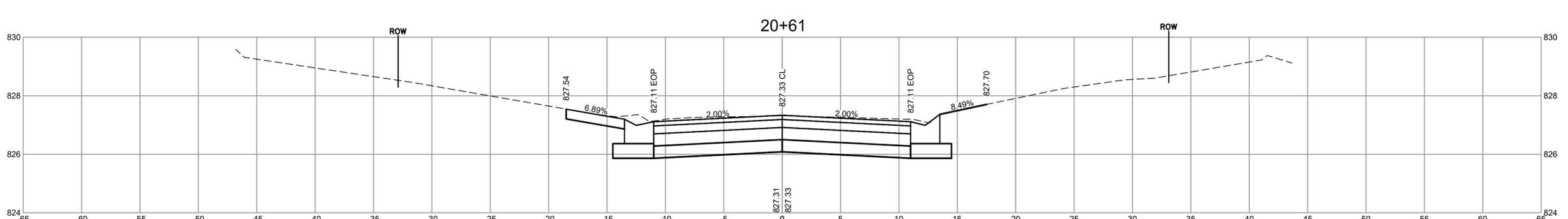
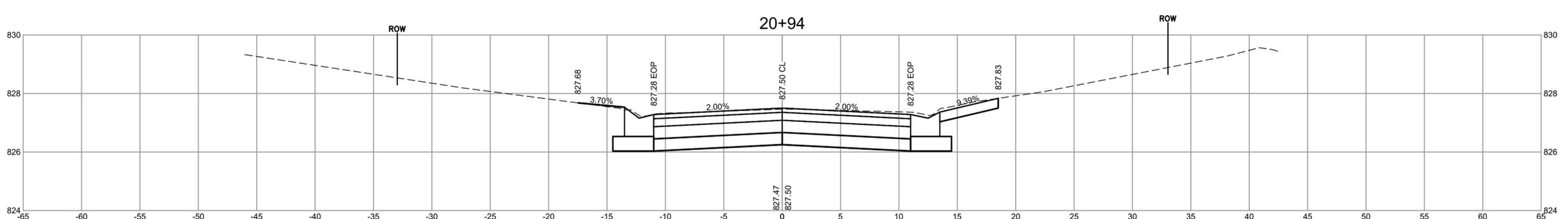
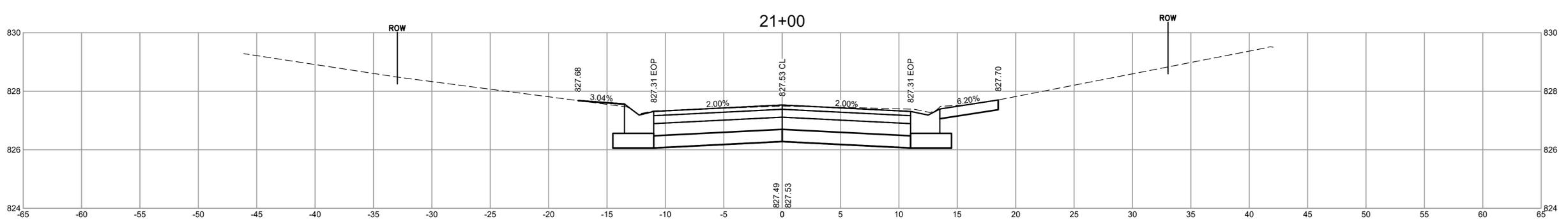
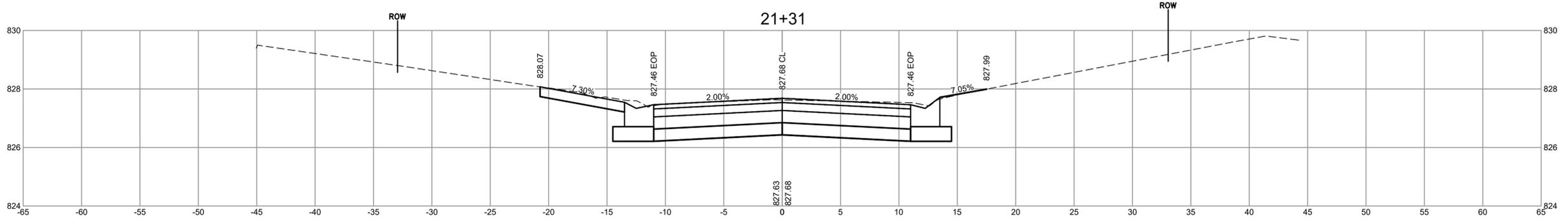
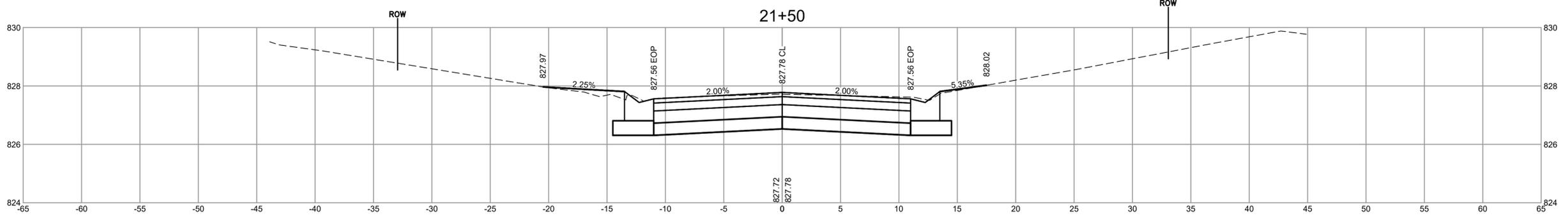
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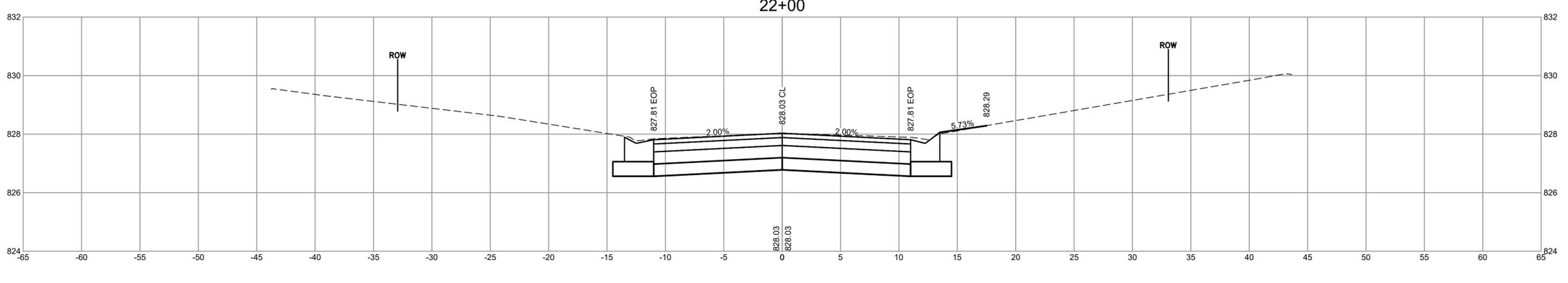
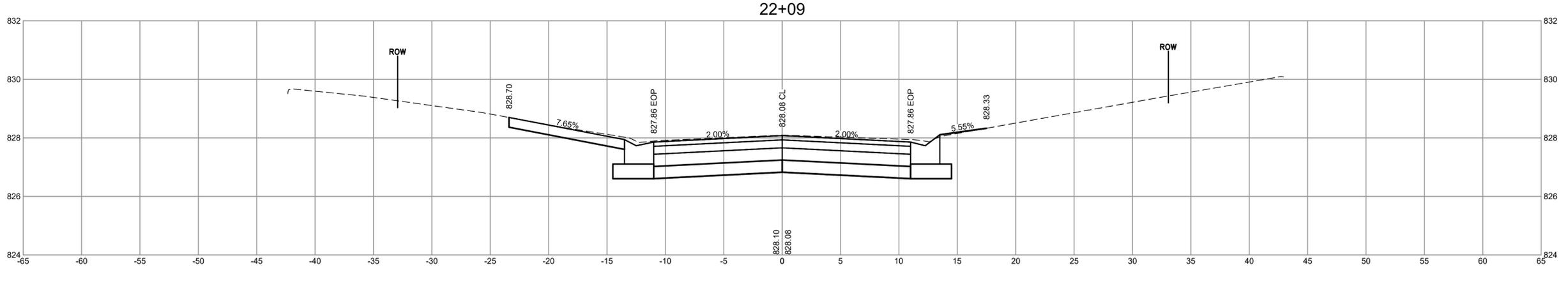
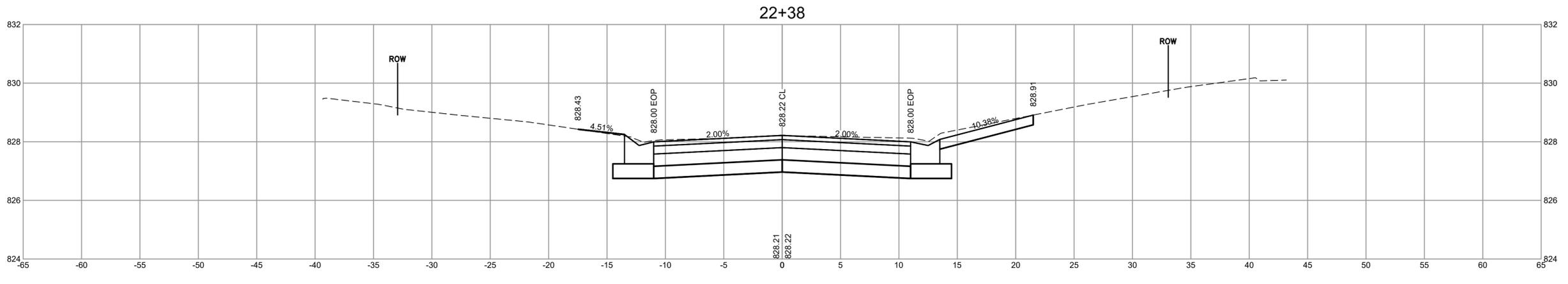
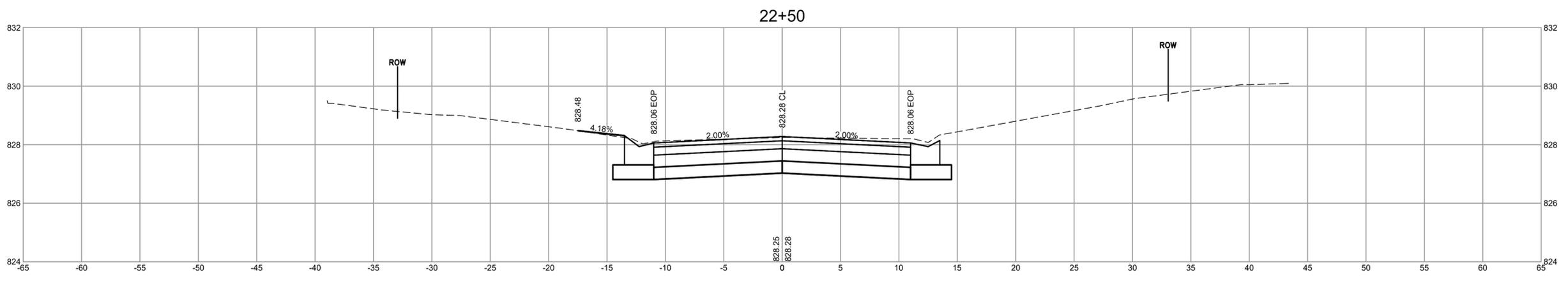
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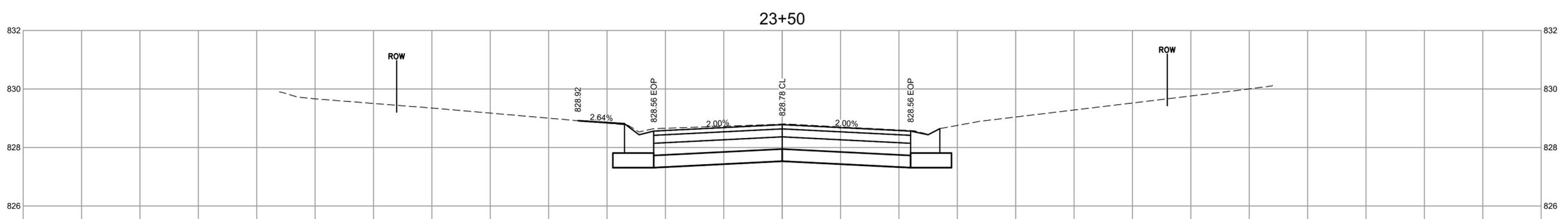
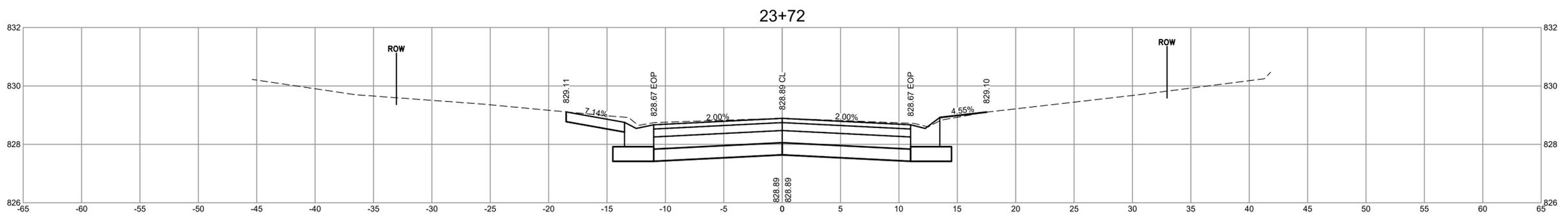
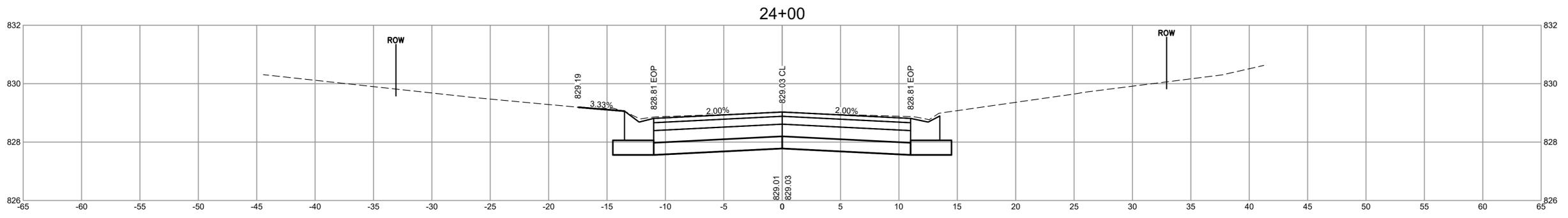
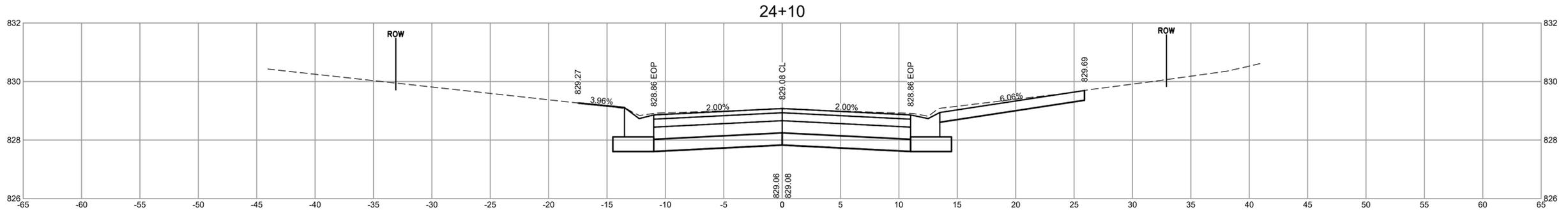
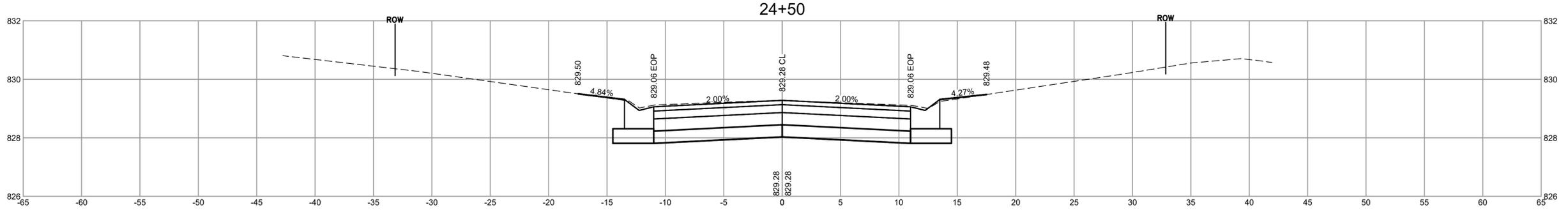
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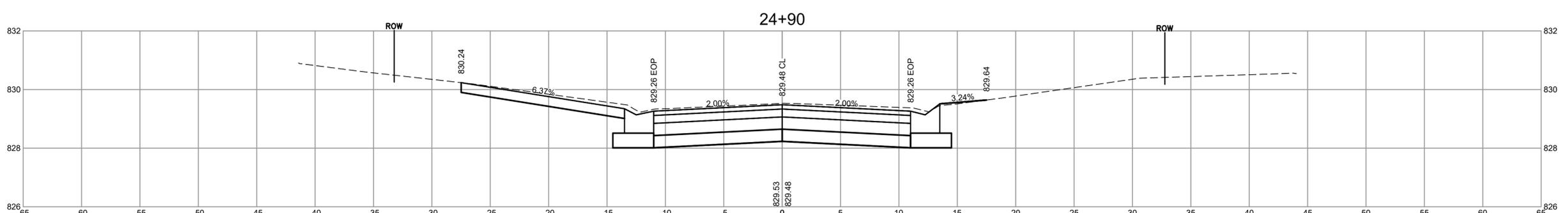
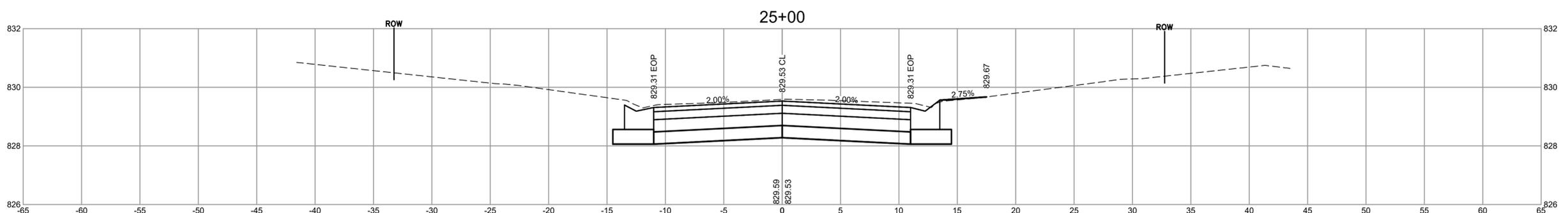
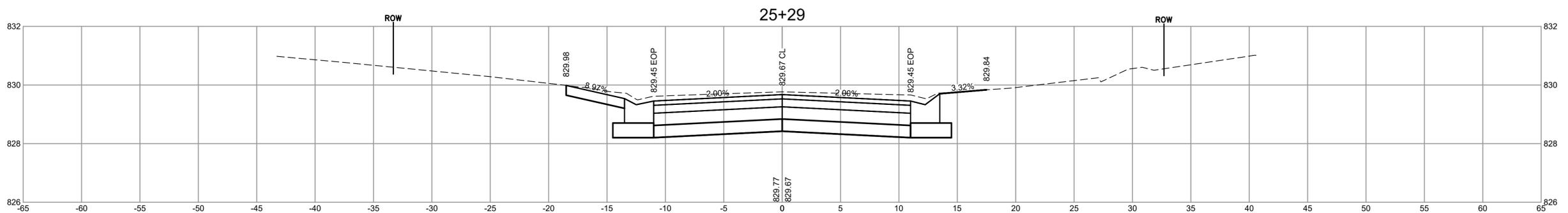
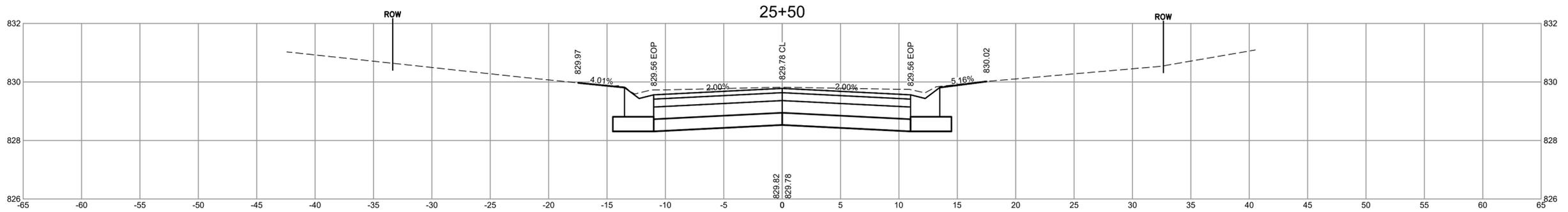
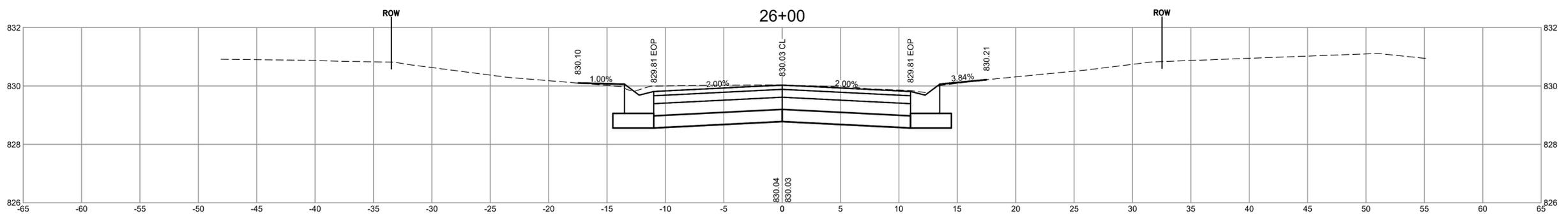
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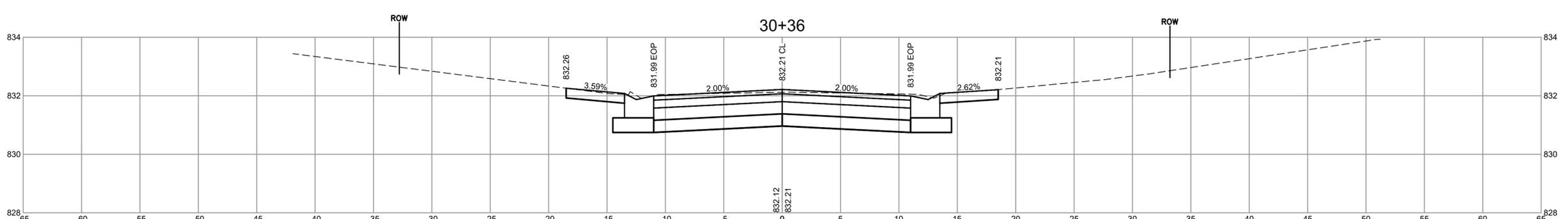
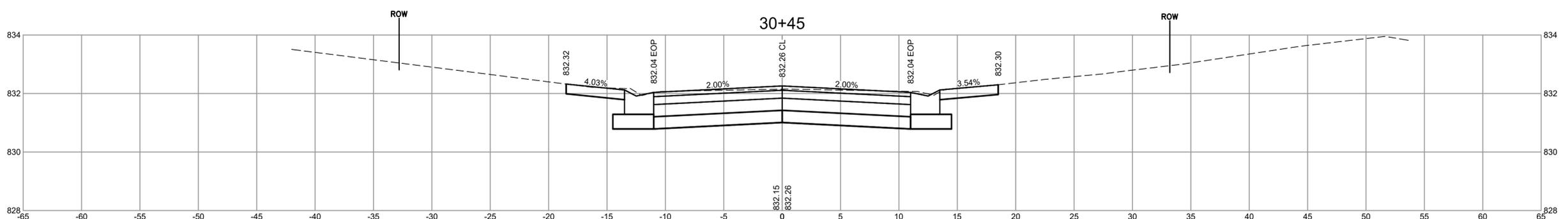
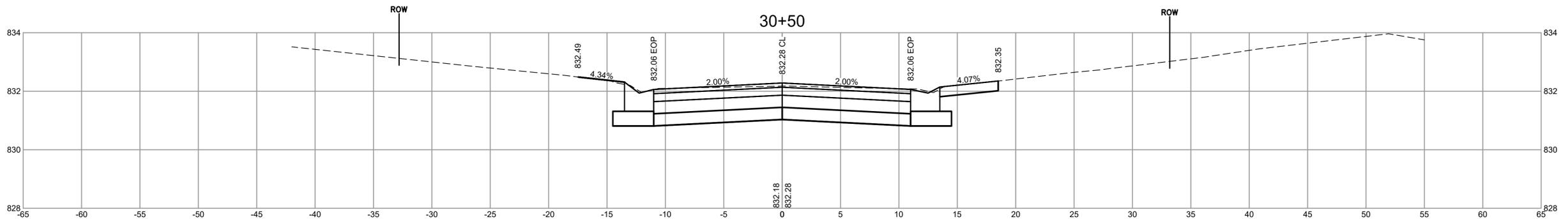
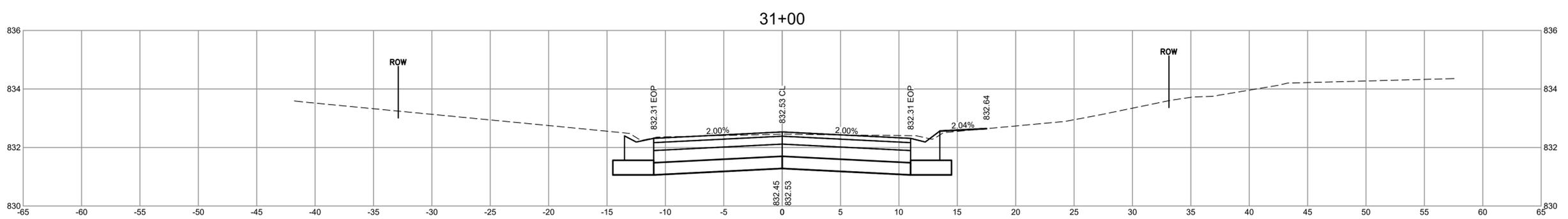
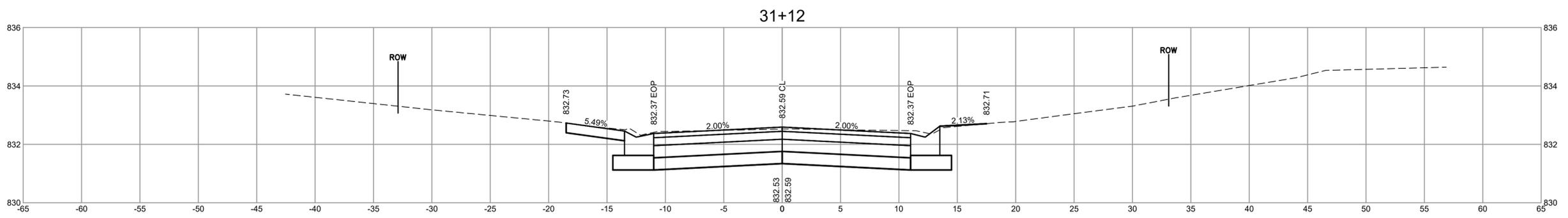
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 GLENWOOD LANE CROSS SECTIONS  
 CITY OF PEWAUKEE  
 WAUKESHA COUNTY, WISCONSIN

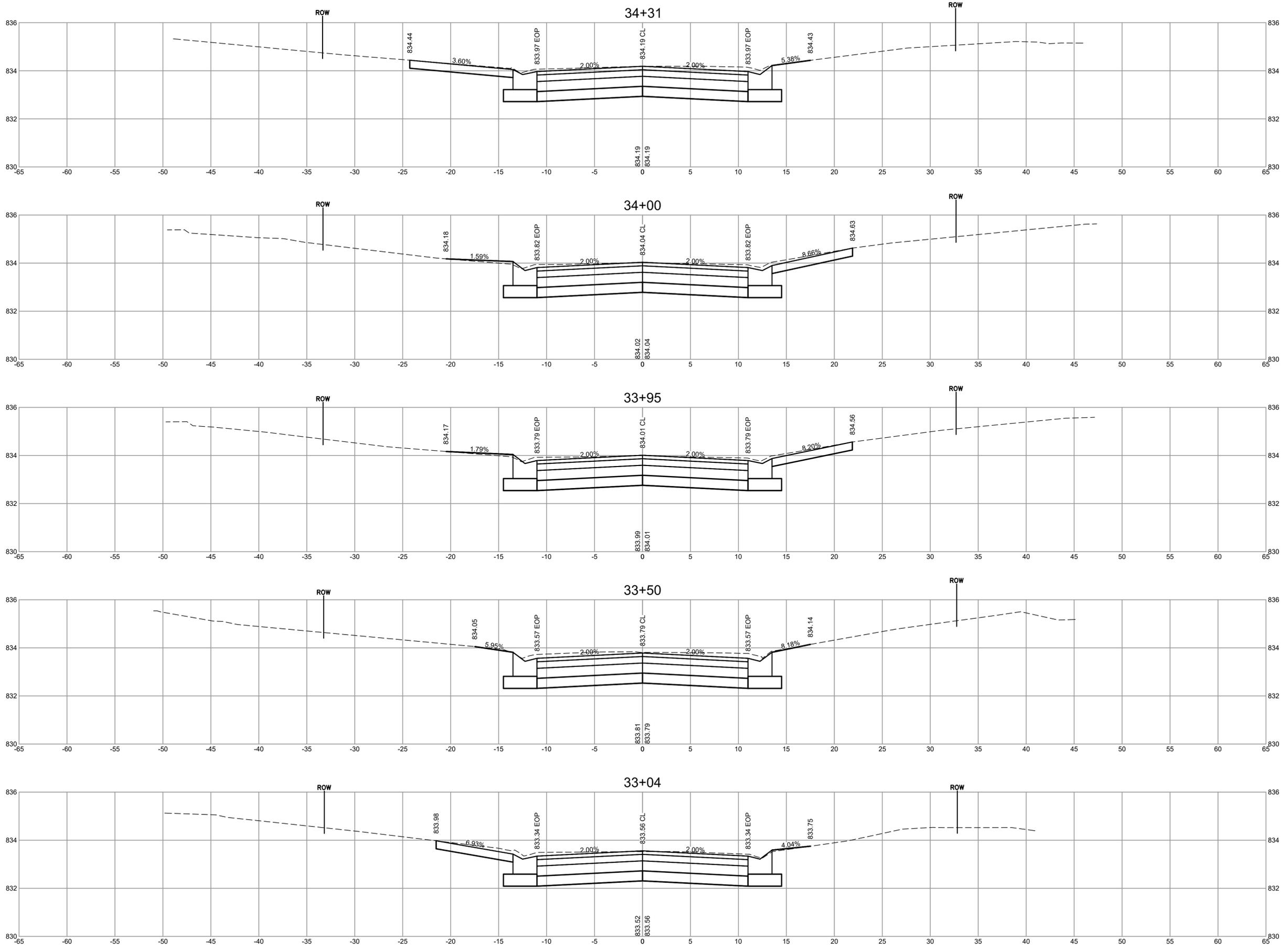
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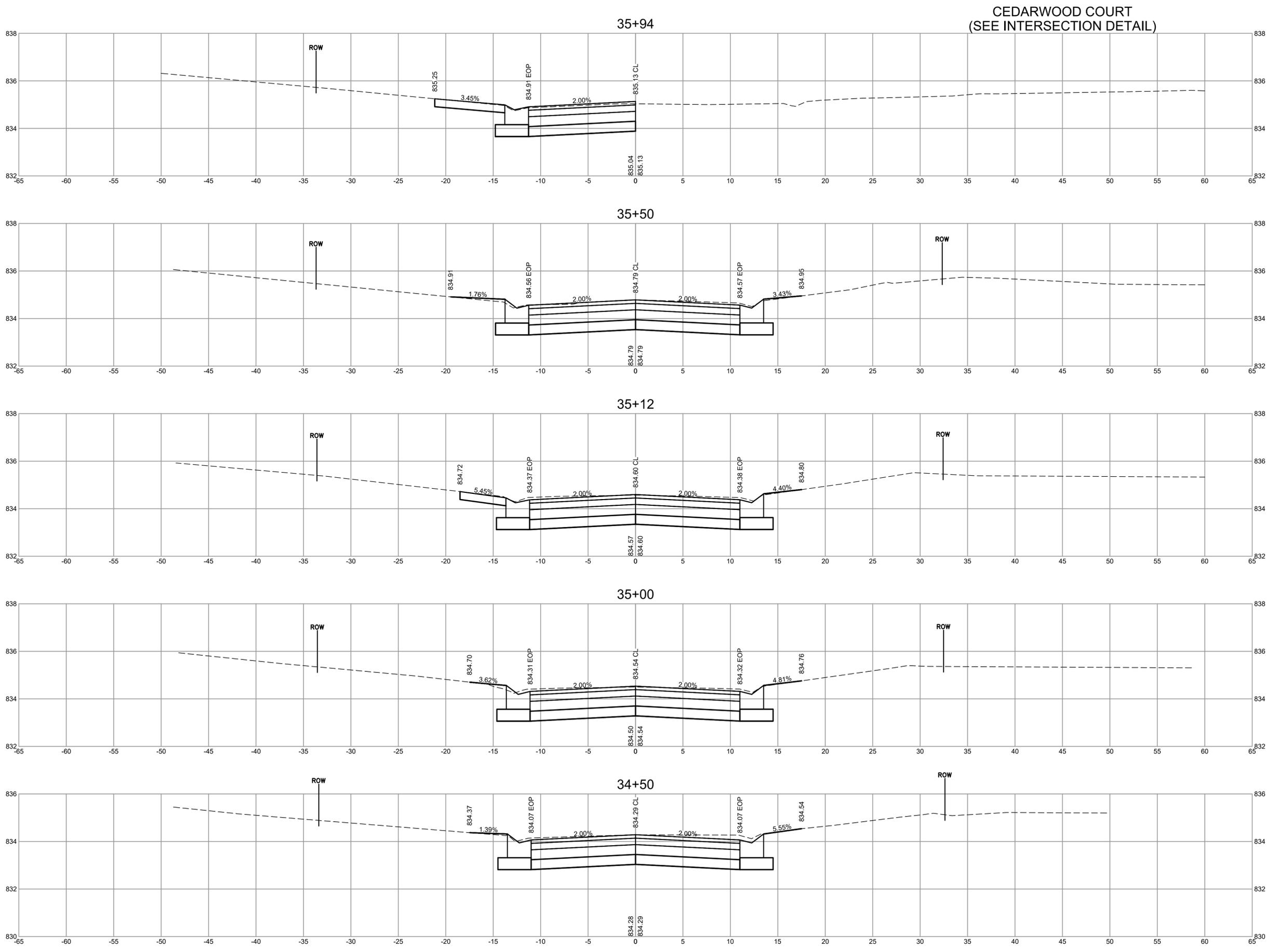
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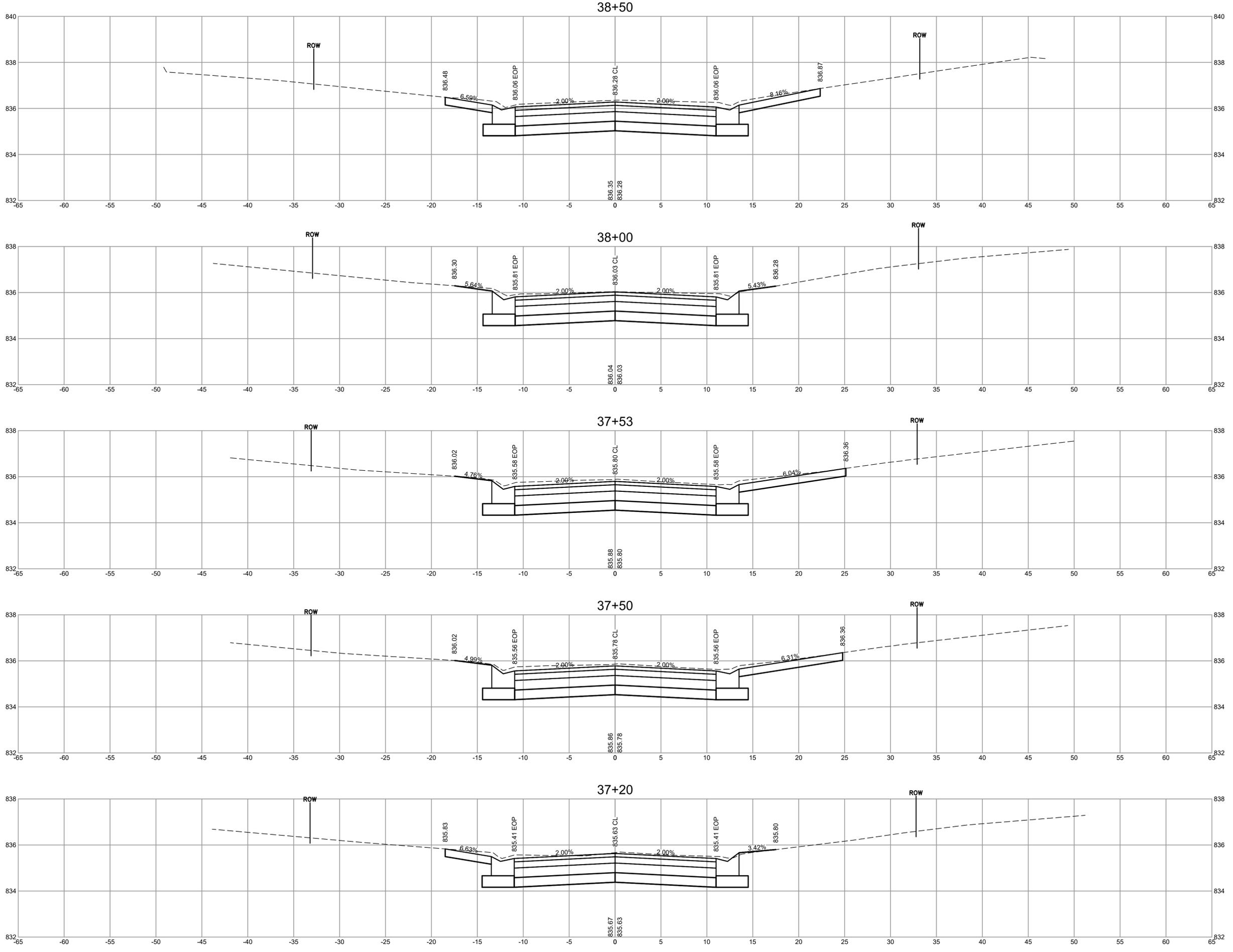
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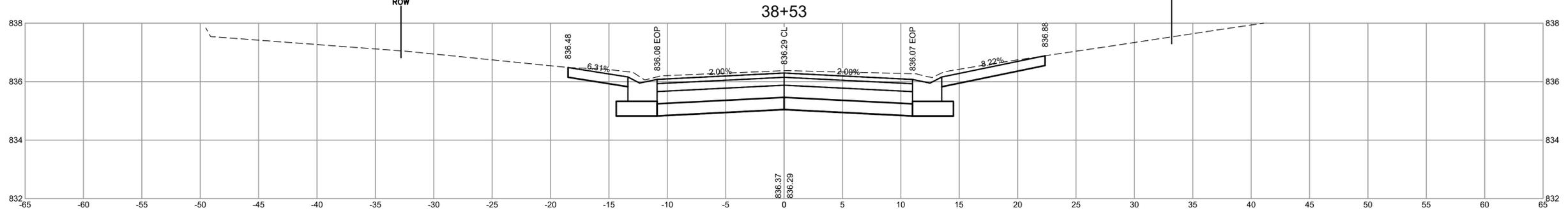
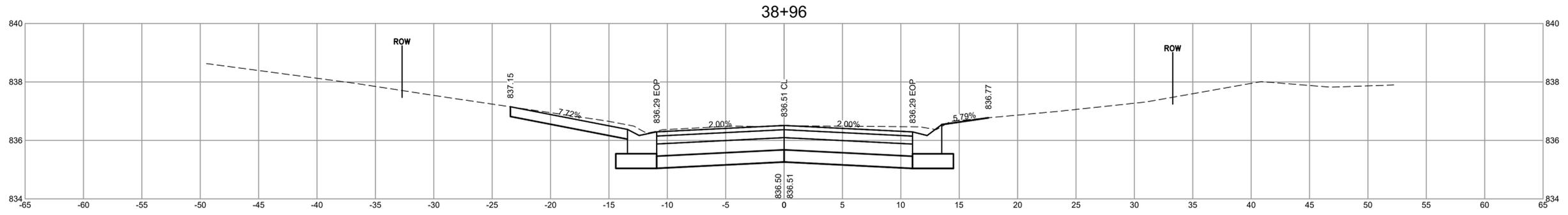
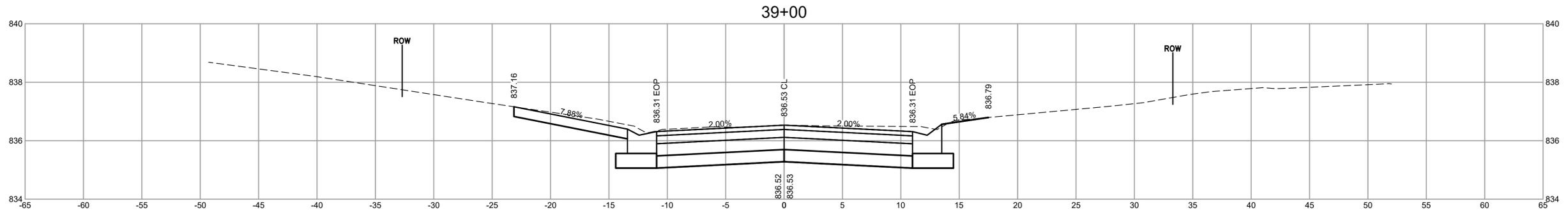
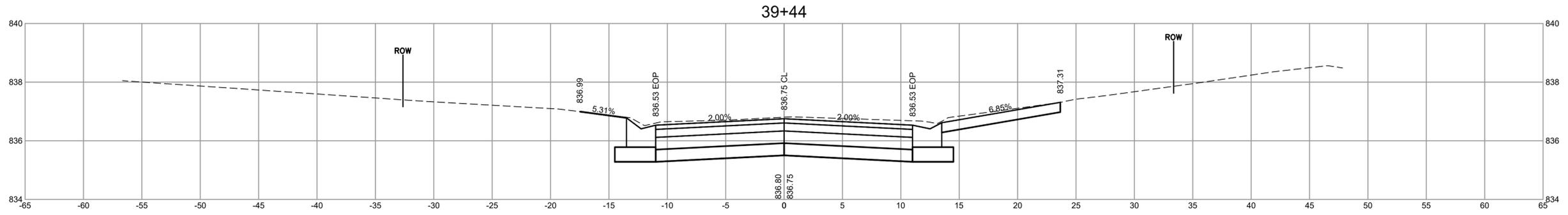
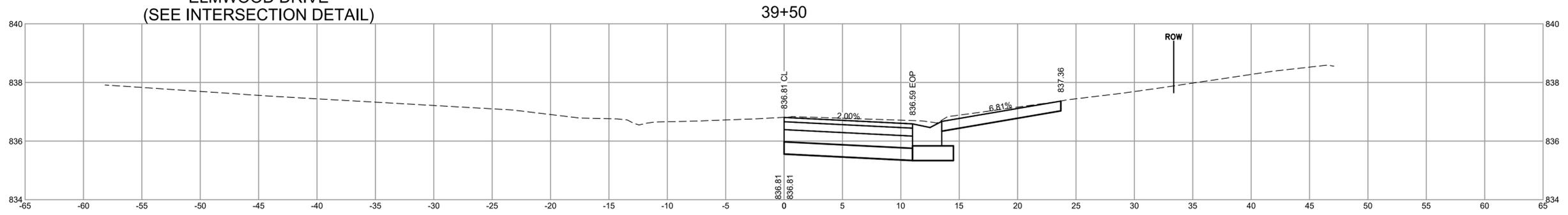
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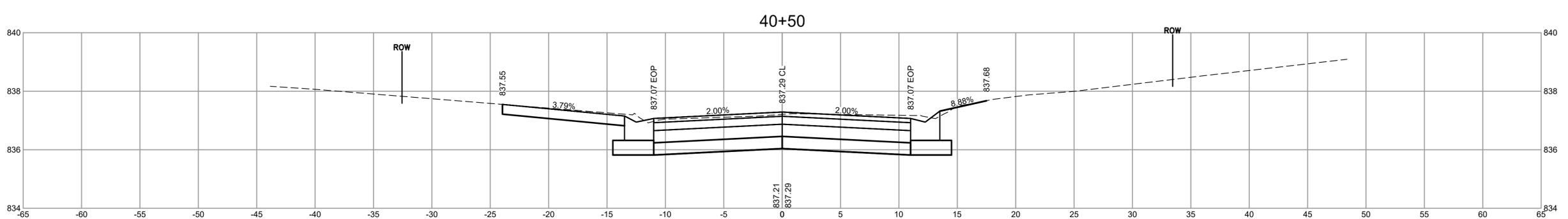
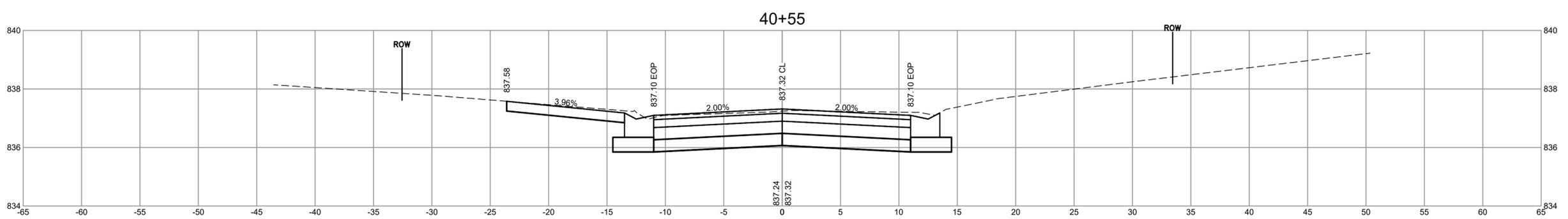
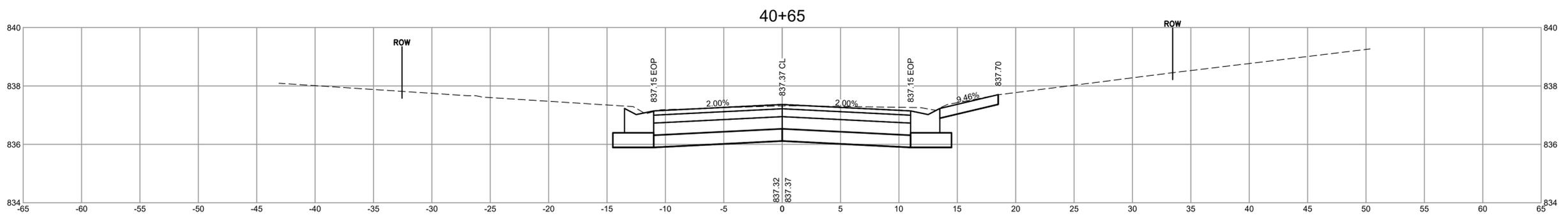
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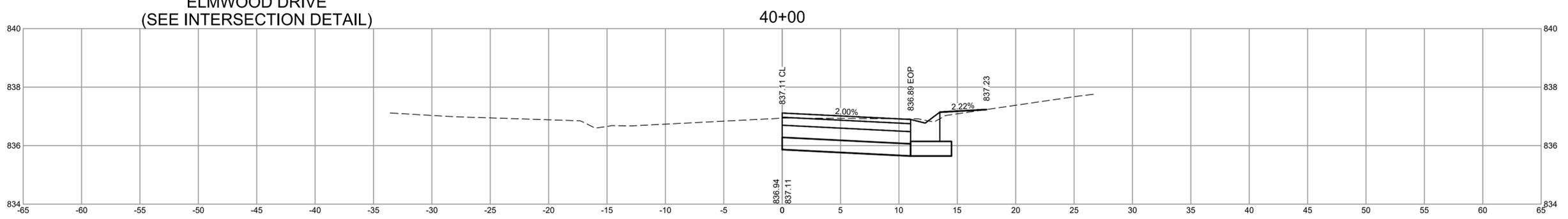
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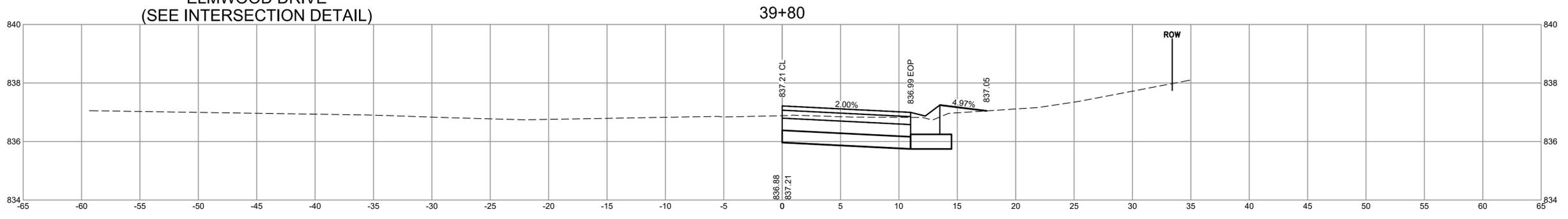
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ELMWOOD DRIVE  
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ELMWOOD DRIVE  
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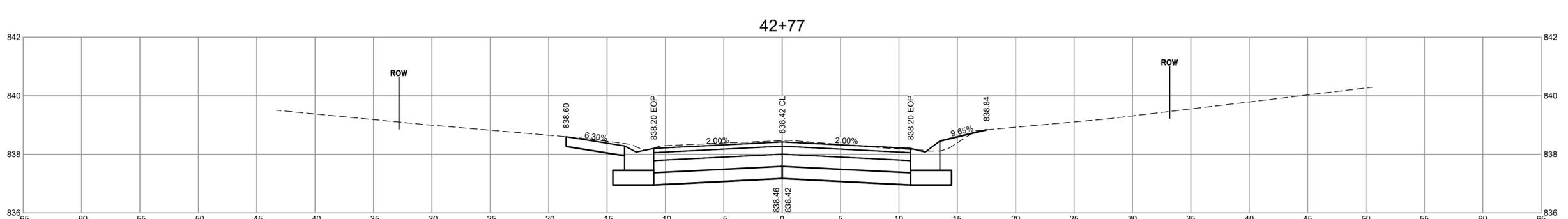
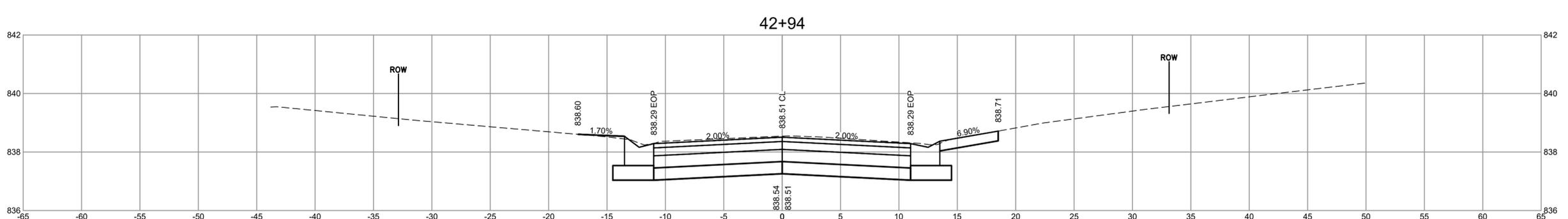
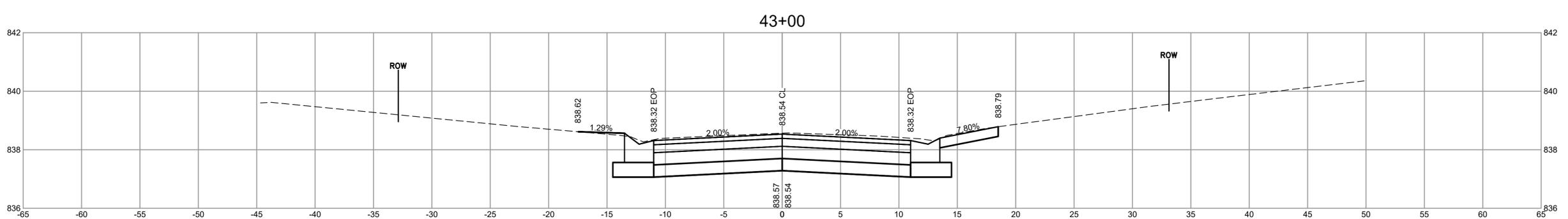
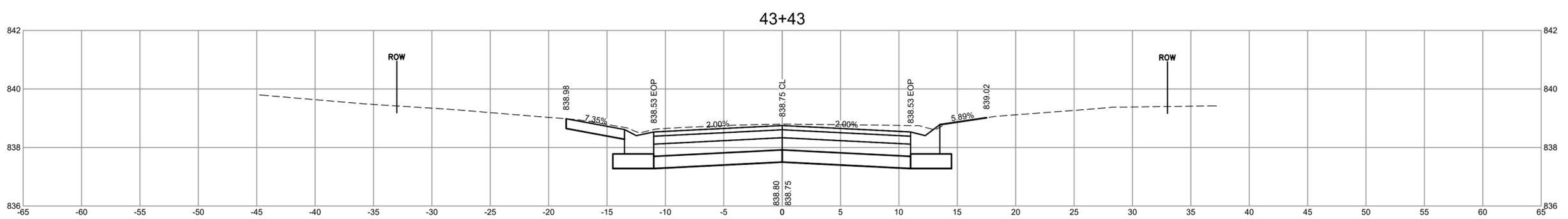
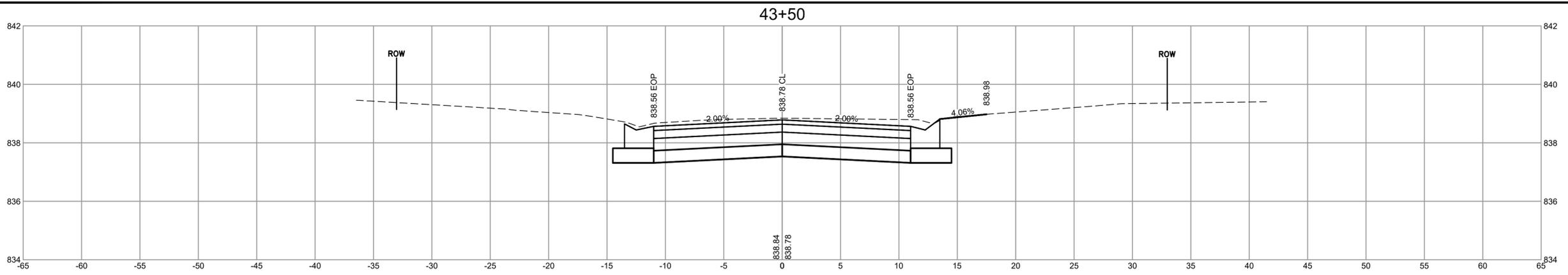
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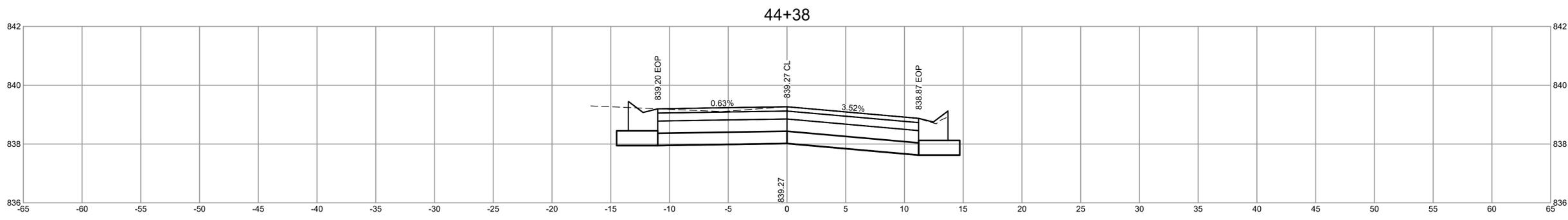
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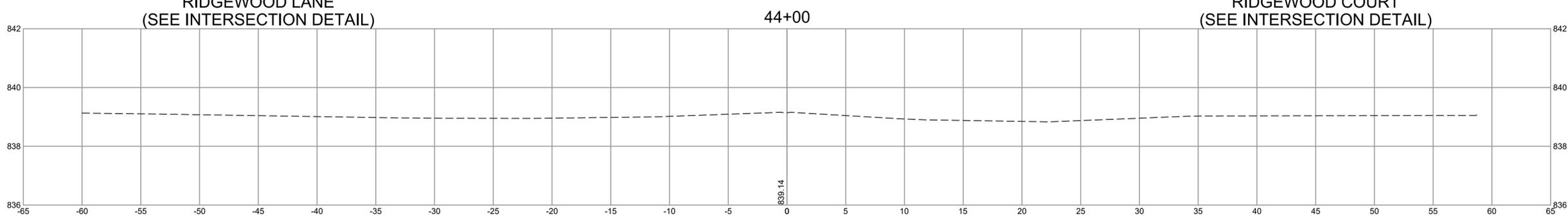
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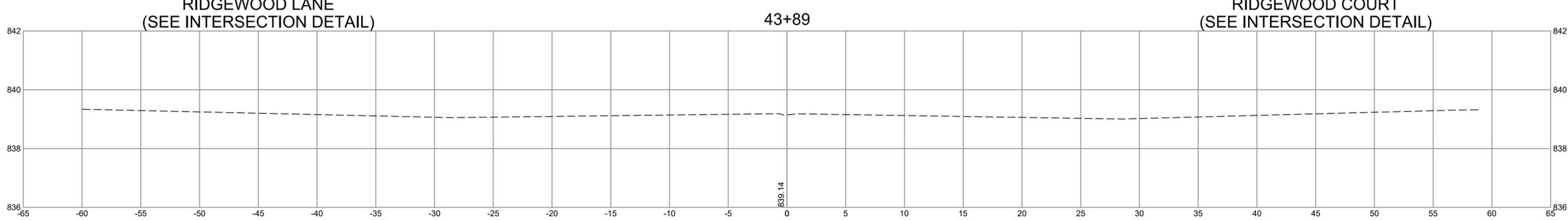
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